



E

ast National Avenue

Corridor Regulating Plan

City of New Berlin



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1

Background



Purpose of the Plan

This plan will guide the City of New Berlin in successful development and redevelopment along the East National Avenue Corridor. A regulating plan for East National Avenue was identified as part of the city's 2020 Comprehensive Plan. This plan also seeks to build off of and complement the West National Avenue Plan completed in 2000, the City Center Plan completed in 1999, and the Comprehensive Plan, adopted in 2009.

The East National Avenue Corridor plan provides a current conditions analysis and case study analysis of different exemplary corridor redevelopments. Also, this plan provides future development options for selected areas along the corridor, as well as locations for improved multimodal access. Overall design guidelines incorporate stormwater best management practices and high-quality urban design characteristics for the corridor.

The successful implementation of this plan will provide the city with detailed examples and the necessary regulatory tool for shaping both future public and private development within the corridor.

Description of the Project Area

East National Avenue is a major east-west corridor in the City of New Berlin. The 3.6 mile corridor begins at Calhoun Road on the west and ends at 124th Street on the city's eastern limit. As the city's primary commercial corridor, the success of East National Avenue is vital to the economic vitality of New Berlin.

The goal of this plan is to create a single unifying image for the entire commercial corridor. However, in order to accomplish this, a series of regulating zones and specific node design concepts have been established in order to concentrate redevelopment on high impact areas. For the purpose of this plan, a node is defined as an area of high concentration of activity. Activity can be defined by high vehicle or pedestrian traffic or a high amount of retail concentration. These three nodes will help to define the entire corridor and provide a foundation for the future redevelopment of properties along East National Avenue.

While part of the same corridor, each node presents a unique set of challenges and opportunities for future redevelopment. The three design nodes that are established for the corridor are the Coffee Road Node, Sunnyslope at National Avenue and the Western Gateway Node.

The Regulating Plan in Chapter 5 includes specific Node Designs for three areas, detailing redevelopment over time.

Coffee Road Node

The Coffee Road Node is located in the center of the East National Avenue Corridor, extending from the intersection of Moorland Road and National Avenue to Coffee Road and National Avenue. Although formally part of the City Center Development Plan for the City of New Berlin, it has yet to be redeveloped into the commercial uses the plan envisions. Coffee Road is currently a rural roadway, meaning it lacks curb and gutter and has no sidepaths. (As of publication, plans are enacted for Coffee Road improvements). Current uses along the road include single family housing and small commercial operations.

Sunnyslope at National Avenue Node

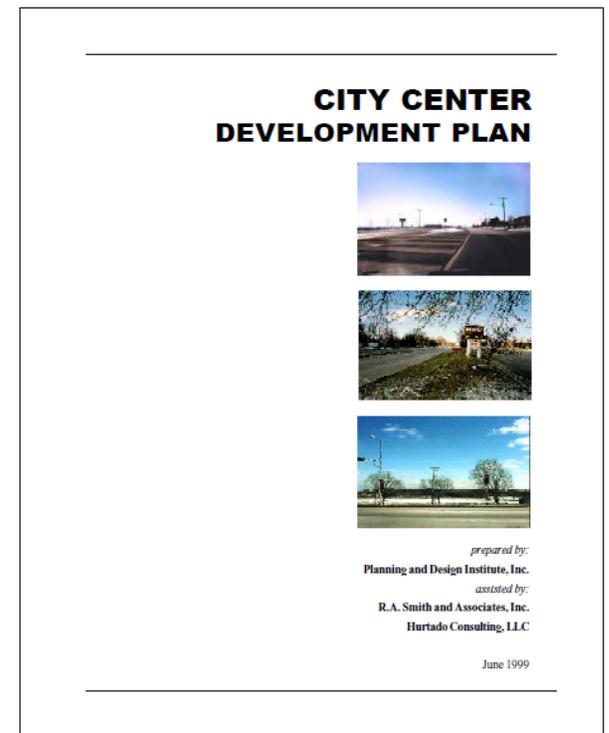
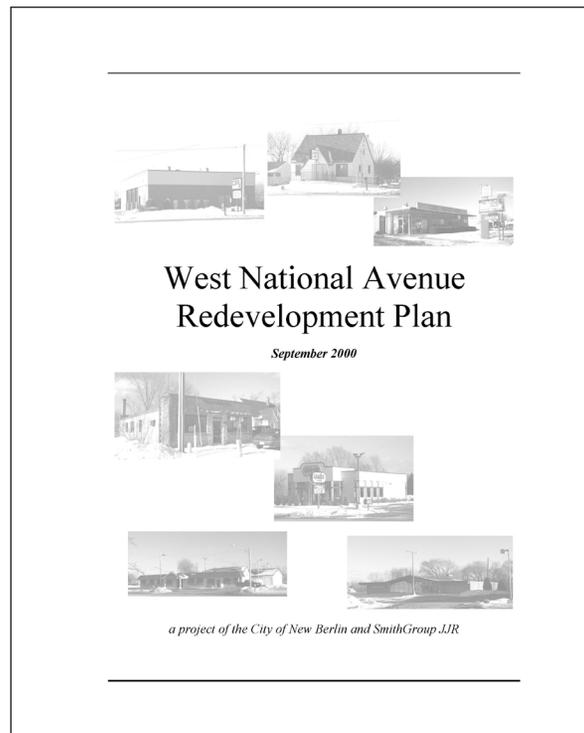
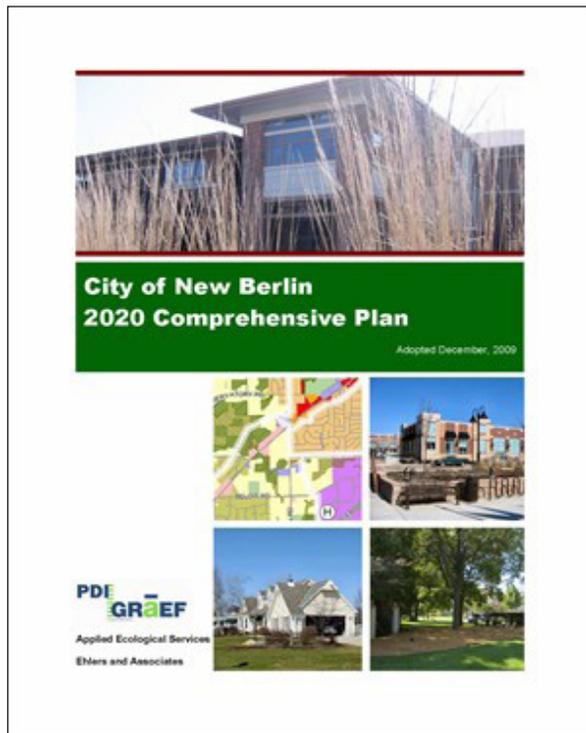
The Sunnyslope at National Avenue Node is located in the eastern portion on the corridor. Although this node shares some of the same commercial aspects as Coffee Road, it is largely a node of office uses. Residential uses also surround the node on the north and south, with many of the city's multifamily housing units found just to the east of the study area.

Western Gateway Node

The Western Gateway Node is the west entrance to the New Berlin commercial corridor and is located between Calhoun and Observatory Road. Calhoun Road is the boundary between the rural, western half of New Berlin and the more urban, eastern portion of the city. Current uses include single family housing, a lumber yard, garden center, and several small scale commercial buildings.

Previous Plans

The City of New Berlin has historically engaged in both short and long term strategies for the development and redevelopment of the community. Through this process, several plans have been created which impact the East National Avenue Corridor. This section provides a summary of each plan and its impacts. The related plans are the New Berlin 2020 Comprehensive Plan (2009), the West National Avenue Corridor Plan (2000) and the City Center Development Plan (1999).



2.1 New Berlin 2020 Comprehensive Plan

The Comprehensive Plan guides future development and redevelopment, while also including an evaluation of the City of New Berlin's cultural, social, physical and social assets and opportunities. The plan encompasses public participation, economic development, agriculture and natural resources, transportation, housing, utilities, a land use plan and a breakdown of each neighborhood. The following goals are highlighted in the plan:

- Build upon strengths of individual neighborhoods
- Provide a predictable regulating process,
- Optimize long term value of public and private investment
- Generate consensus among residents, business and property owners

The East National Avenue Corridor Regulating Plan intends to build upon these goals and create a cohesive building fabric among businesses along East National Avenue. Specifically, the comprehensive plan discusses Neighborhood E (National Avenue East Corridor) and the need to create a plan for future development. This Regulating Plan echoes many of the goals and objectives detailed within the Comprehensive Plan, such as continuing "public investment in streetscape improvements, including landscaping, street lighting, sidepaths and bike trails."

2.2 West National Avenue Plan

The city of New Berlin created the West National Avenue Redevelopment Plan in 2000. This plan includes National Avenue from South Town Road to South Moorland Road. The following goals are highlighted in the plan:

- Attract commercial and office uses
- Redevelop economically or structurally obsolete buildings
- Enhance corridor aesthetics while maintaining neighborhood compatibility, and
- Protect property values and enhance the tax base

The plan provides several recommendations for the corridor. These recommendations include suggestions on land use and zoning changes, general design guidelines and three specifically designed sites. In addition, there are recommendations on which parcels should be consolidated to provide cost effective development patterns. Transportation recommendations were also included to enhance the safe and efficient movement through the corridor.

Like the West National Avenue Redevelopment Plan, the East National Avenue Corridor Regulating Plan will examine many of the same issues. The East National Avenue Plan will be used as a starting point and builds on the recommendations made in the West National Avenue Plan. While the East National Avenue Plan makes many of the same recommendations, it also expands on sustainable design measures and regulations.

2.3 City Center Development Plan

The City Center Development Plan was created due to increasing traffic and development pressures near the intersection of National Avenue and Coffee Road. The City Center Development Plan consists of development and urban design concepts and guidelines. City staff, residents, committee members and property owners/managers participated in the creation of these concepts. The plan outlines the following objectives:

- Create an identity for the City Center
- Provide a safe environment for vehicles and pedestrians
- Integrate pedestrians, bicyclists and automobiles
- Recommend a long-range plan for development in and around National Avenue and Moorland Road

The City Center Plan also addresses "Visual Character", detailing how there should be a strong visual edge along right-of-ways, uniform setbacks and attractive signage. Like the City Center Plan, the East National Avenue Corridor Regulating Plan incorporates design concepts for each of the three design nodes. This Regulating Plan also encourages the types of visual enhancements found in the City Center Plan, contributing to a cohesive building fabric. It also addresses the issues of parking, drainage and landscaping as was discussed in the City Center Plan. Node designs include a balanced strategy to the development and redevelopment of the area discussing how parking, drainage and landscaping can be implemented effectively.



Problem Statement

National Avenue is the primary, mixed-use corridor and gateway to the City of New Berlin, Wisconsin. Irregular setbacks and pockets of development have left the corridor without a cohesive building fabric along National Avenue. Furthermore, some of the existing building stock and parking design are outdated for current business needs. The success of the corridor is crucial to the economic vitality and long-term sustainability of the corridor and city.

Goals and Objectives

Goals, objectives and criteria are detailed expressions of a community's ambitions for the future. They are derived from researching community strengths, weaknesses, desires, and needs. Goals and objectives help drive the planning process and the implementation of the plan. For the purpose of this regulating plan, goals, objectives and criteria were formulated through research of previous plans, discussions with city staff and input from community members during the public participation process.

A goal is a description, in general terms, of broad aims and desired achievements. It is a statement of aspirations which indicate broad social, economic, or physical conditions which a community seeks to achieve. Goals are typically broad and long-range.

An objective is a statement of a desired course of action. It is more specific than a goal and describes a course of action in order for that goal to be reached. Objectives help direct actions that are necessary to achieve community goals.

A specific course of action must be taken in order to fulfill a desired outcome. Criteria help to ensure that goals and objectives will be met during the implementation of a plan.

Goal:

Develop a regulating plan which creates a cohesive, mixed-use corridor with an identity along National Avenue.

Objective 1

Enhance the appearance of the corridor for more cohesive development which creates a unique identity for East National Avenue.

Criteria:

1.1 The proposed recommendation must guide (re)development to be consistent with Chapter 15 of the New Berlin 2020 Comprehensive Plan.

1.2 The proposed recommendation must have consistent setback requirements to promote uniform building placement.

1.3 The proposed recommendation must be consistent with current City of New Berlin streetscaping and landscaping requirements.

1.4 The proposed recommendation must use high quality building materials, such as masonry, in order to create quality designed, permanent structures in the corridor.



Objective 2

Retain and attract businesses that add value to the corridor.

Criteria:

- 2.1 The proposed recommendation will respond to current and anticipated market demand.
- 2.2 The proposed recommendation must enhance the city's tax base by encouraging redevelopment of underutilized properties in the corridor.
- 2.3 The proposed recommendation must protect and preserve existing property values.

Objective 3

Create an environmentally sustainable suburban commercial corridor.

Criteria:

- 3.1 The proposed recommendation must protect streams, woodlands, important viewsheds and primary and secondary environmental corridors (as defined in the New Berlin 2020 Comprehensive Plan).
- 3.2 The proposed recommendation must incorporate elements of green building practices, as recommended by LEED or a LEED equivalent rating system.
- 3.3 The proposed recommendation will not increase demand for off-site stormwater treatment.

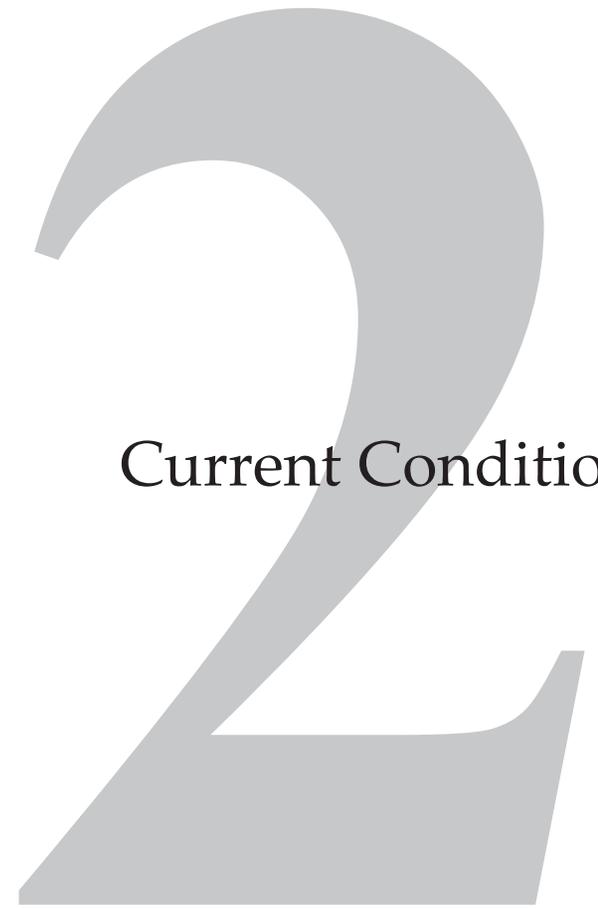
Objective 4

Enhance efficiency of both pedestrian and vehicular movement and access throughout the corridor.

Criteria:

- 4.1 The proposed recommendation must provide safe pedestrian crossings while maintaining traffic flows.
- 4.2 The proposed recommendation must eliminate or consolidate street curb cuts by internally linking or combining parking lots to improve circulation.
- 4.3 The proposed recommendation must accommodate pedestrian movement by incorporating walkways that increase linkages in the corridor to surrounding residential areas.





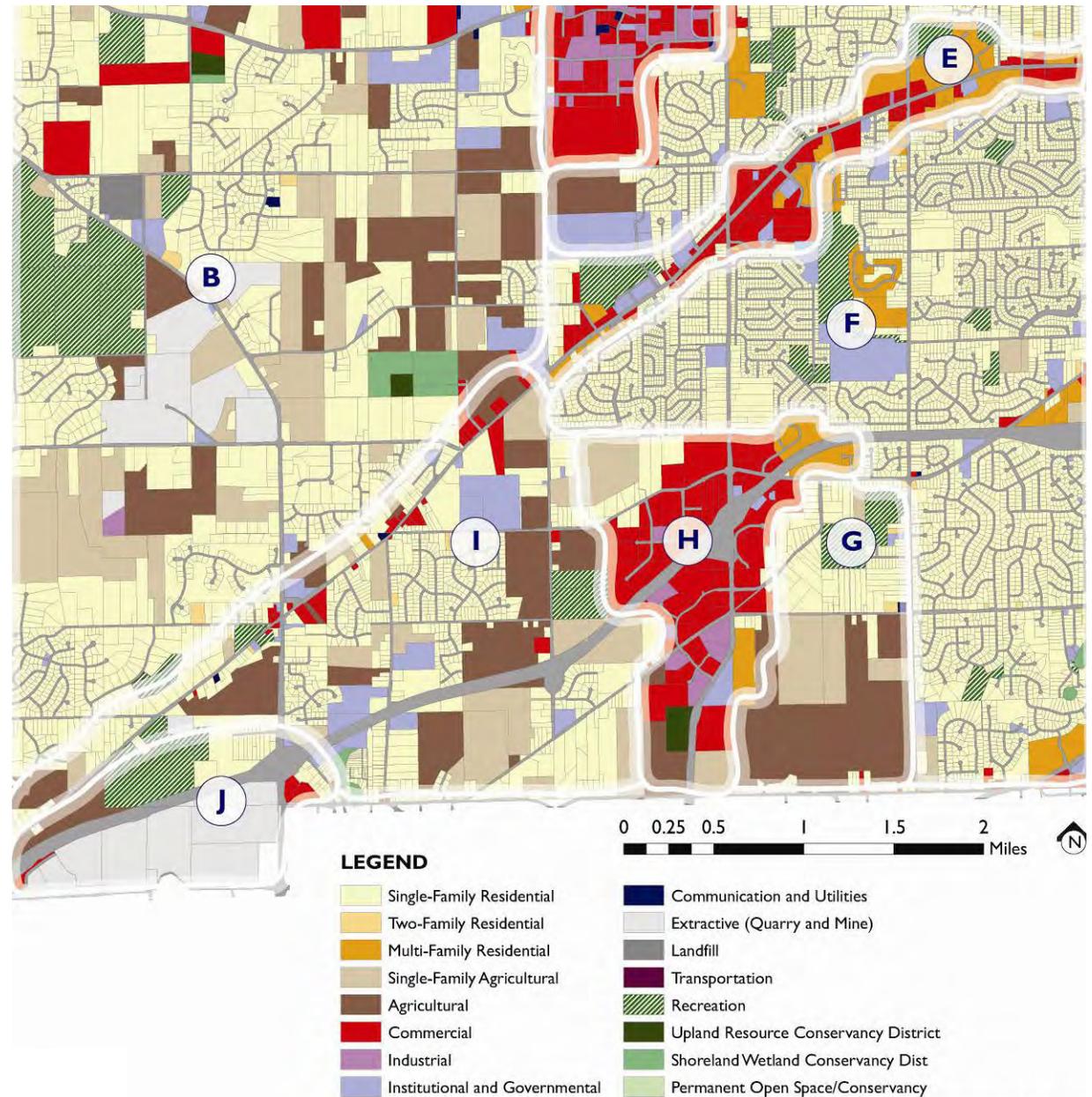
Current Conditions



2.1 Current Land Use

Currently, the majority of the East National Avenue Corridor consists of commercial uses. An assortment of commercial uses have developed throughout the corridor over the years and vary in scale from small retail shops to large big-box retailers. Commercial uses located within the corridor include fast food restaurants, offices, banks, gas stations, convenience stores and auto services. Multi-family dwellings are also present throughout the corridor--specifically near the Sunnyslope Road and National Avenue intersection. Bordering the corridor to the north and south are numerous single-family houses.

The Western portion of the Corridor is characterized by many single-family homes, a few multi-family units and small strips of commercial uses. The Civic Center area located near the intersection of Casper Drive and National Avenue is home to many governmental functions including the City Hall and Police Department. From Moorland Road to Coffee Road a cluster of retail establishments have developed on both the northern and southern sides of National Avenue. The City Center development is within this portion of the corridor and, in addition to retail establishments, is home to the City's Library. The eastern portion of National Avenue, characterized by a mix of commercial uses, is less dense than the central portion of the corridor. Development in this area is scattered along National Avenue with a few vacant parcels or opportunities for future infill development.



2.2 Social and Economic Activity

Social and economic activities change greatly throughout the corridor. Each node differs in terms of activity, access and building size.

Coffee Road

The Coffee Road Node has a significant amount of commercial development. A large retail stripmall located along Moorland Road, in conjunction with several fast food establishments in this node, have resulted in a high concentration of vehicular traffic. For many retail-based businesses, adequate access is an important consideration when choosing a location for their business. The Coffee Road node, as well as the neighboring City Center, both have easy access to I-94 via Moorland Road.



Sunnyslope Road

The Sunnyslope Road Node is characterized by several retail establishments including a flower shop, music store, gas stations, banks and a large big-box grocery store. Numerous parcels within the node are currently undeveloped. Many activities in this node abut single-family residential subdivisions. Future development in this area should focus on infill development along East National Avenue.



Western Gateway

The Western Gateway Node is predominantly rural, as it is the least developed area of the East National Avenue Corridor. It is characterized by several single-family homes and a few multi-family housing units. A cluster of commercial buildings exists at the eastern edge of Observatory Road. Businesses within this commercial cluster include a landscape nursery, funeral home and a hair salon.



2.3 Vehicular Access

Cars are the predominant mode of transportation throughout the City of New Berlin. National Avenue is a major arterial into and through the City. Important entry points into the Corridor include the intersections of 124th Street, Sunnyslope Road, Moorland Road and Calhoun Road. Numerous minor arterials also serve National Avenue and connect the many residential neighborhoods to the Corridor. Traffic flow from numerous curb cuts, parking aisles and lanes impede the traffic flow on National Avenue. Vehicular access problems often arise while turning onto National Avenue. Given the large scale of roadways and intersections, traffic along National Avenue travels at high speeds, increasing the potential for accidents and prohibiting pedestrian mobility.



2.4 Pedestrian Access

The entire East National Avenue Corridor incorporates sidepaths along both sides of the streets. However, many of these sidepaths do not extend into adjacent neighborhoods and are discontinuous along the roadway. A lack of available pedestrian crossings within the corridor compound access problems. Residential units are cut-off from community parks and civic buildings because of the large-scale intersections and roadways, which hinder pedestrians' crossing ability. Overall, the corridor lacks a pedestrian community connectivity.



2.5 Physical Form and Character

The character of National Avenue changes throughout the length of the corridor. The style of buildings reflect the transition from the City's rural west end to the more urban or suburban central and east end of National Avenue. Many buildings along National Avenue were built decades ago and still function as small retail facilities, while new developments are scattered throughout the corridor.

The western end of National Avenue includes a mix of single-family and multi-family residences with some one-story retail mixed in. Buildings are more spread out over larger parcels in this area.

The central part of National Avenue, around Moorland Road, has varying types of retail buildings. Many residential buildings have been converted to storefronts over time. This lends to a disjointed and dated appearance to the corridor. Retail in former residential buildings require façade improvements with higher-quality materials.

Towards the east end of National Avenue, around Sunnyslope and 124th Street, larger developments of healthcare and senior living facilities were built in the last decade.

Overall, properties in the Corridor are well maintained. Generally, buildings are built to one and two-story heights, while newer residential buildings at City Center and the senior living facilities are three to four stories.

2.6 Landscape and Streetscape

Like the building stock along East National Avenue, the streetscaping varies throughout the Corridor. Recent developments typically have coordinated street lights and signage, whereas other stretches of the street do not have street lights. While some new streetlights have attractive banners attached, others do not. Additionally, street trees in the parkway along East National Avenue are not continuous along the length of the corridor.

Landscape treatments also vary in the East National Avenue Corridor. Older parcels in the Corridor do not incorporate landscaping elements while newer developments use extensive landscaping to screen parking lots. In some areas where residential developments abut National Avenue, there is no landscape buffer, such as at Sunnyslope Road. Multi-family developments in the West Gateway node, however, have earth berms and landscape to screen side yards from National Avenue.



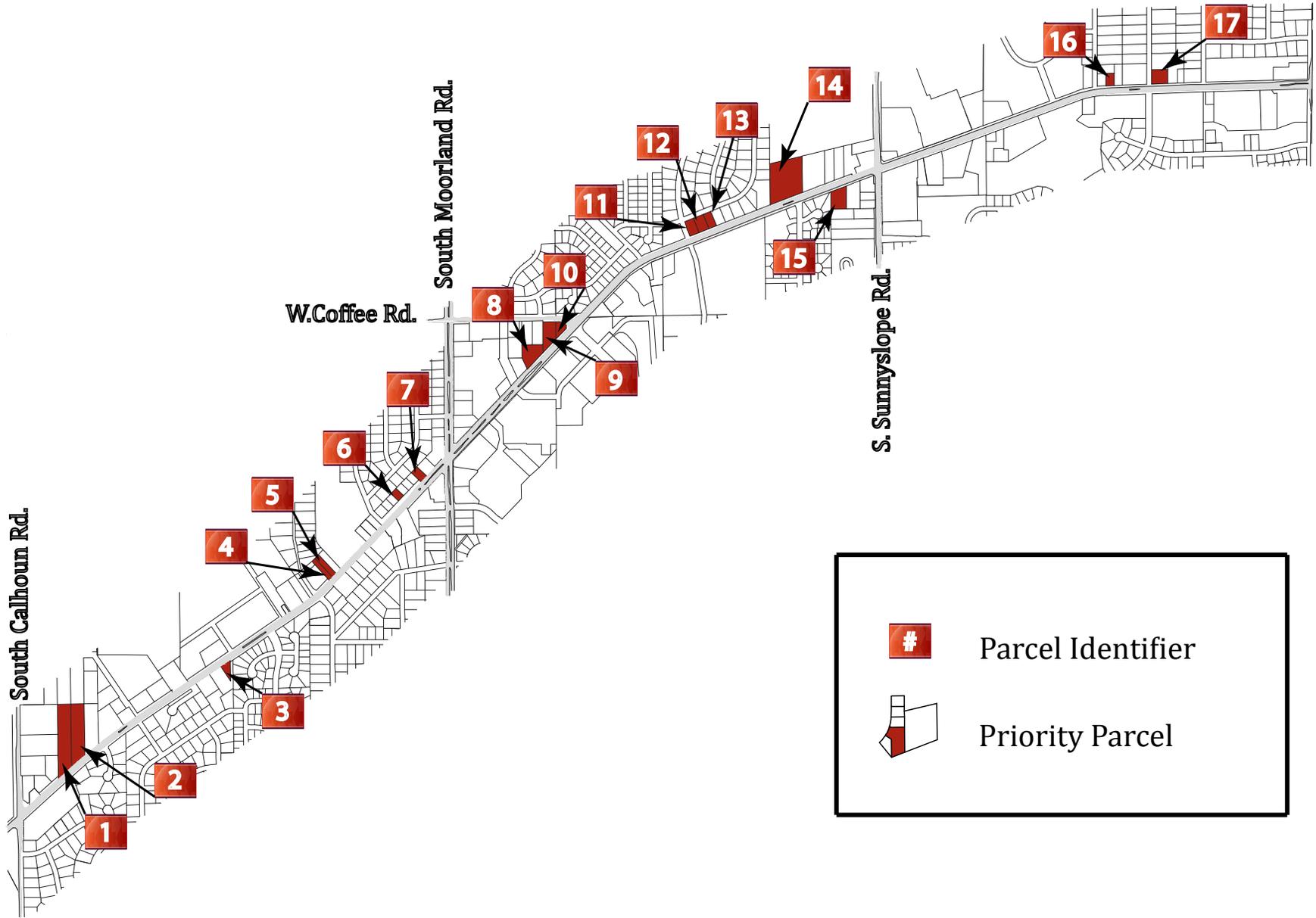
2.7 Priority Redevelopment Parcels

A parcel analysis of the East National Avenue Corridor identifies priority parcels for redevelopment. Priority parcels possess a variety of the following characteristics:

- Small parcels
- Oddly shaped parcels
- Vacant parcels
- Structures that would benefit from design modifications
- Structures and sites that need improved maintenance
- Sites not designed to maximize building and parking areas
- Non-conforming uses

Each selected site is outlined with information including the business name, address, tax parcel key and site area, found in Appendix A.1.

Priority Redevelopment Parcels



3

Public Input

Public participation was a principal consideration throughout the planning process. Various forms of participation were used in order to gain citizen input on design alternatives and planning considerations.



3.1 Citizen Meetings

During the comprehensive planning process undertaken by the city of New Berlin in 2009, several neighborhood meetings were held in order to gain insight on neighborhood strengths, weakness, issues, opportunities, desired land uses, and preferred architectural design character.

Citizen input from these neighborhood meetings were recorded and used to guide the design process and development of alternatives for the East National Avenue Corridor Regulating Plan.



3.2 Survey of Businesses

A survey was sent to all businesses in the East National Avenue Corridor. The intent of the survey was to gain insight into specific corridor issues, such as general opinions of the corridor's appearance, access to parking and availability of land for business expansion. Questions were also asked to gauge a business' willingness to undertake a Business Improvement District, participate in façade improvement programs and interest in marketing the corridor. The survey was sent to 303 business owners in the corridor with a twenty-seven percent response rate. Survey questions and more detailed responses can be found in Appendix A.3.

The following key information was obtained from the survey:

- 71% of expanding businesses want to stay in the corridor
- 73% of respondents claim to have sufficient parking for their needs
- 51% of the respondents are interested in sharing parking
- 51% of businesses in the corridor lease their current location
- 53% of businesses have been in the corridor for 16 or more years

3.3 Neighborhood Open House

A neighborhood open house was held on Tuesday, April 6, 2010 at the New Berlin Public Library. All businesses receiving the survey and all residents in the neighborhood were invited to attend the meeting. The open house was held in order to introduce the Corridor plan project to the community and seek input from residents on node design alternatives. About thirty members of the community attended the meeting to share their thoughts and concerns.

The following information was obtained in regards to the node design alternatives:

- High density development was a concern in the Western Gateway Node
- Residents appreciate the idea of expansion of public parks, sidepaths and the inclusion of recreational trails
- Prioritize future commercial development at City Center
- Residents welcome the opportunity for public interaction at City Center Node
- Concerns were raised over pedestrian crossings along National Avenue, due to the high traffic volume

3.4 Presentation to Plan Commission

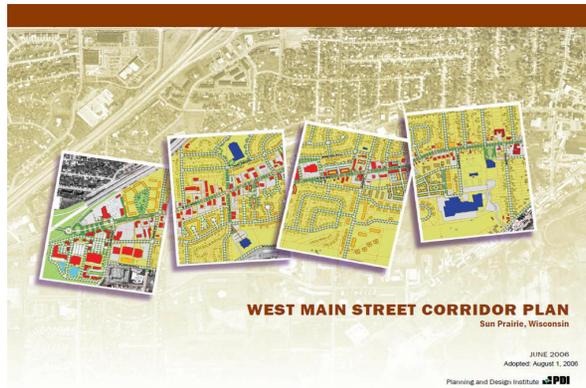
A draft of the East National Avenue Corridor Regulating Plan and conceptual node designs were presented to the New Berlin Planning Commission on Tuesday, May 3rd, 2010 at the New Berlin City Hall.



4

Case Studies

In order to determine which methods can help address the issues facing East National Avenue, three case studies were examined. Successful models of regulations and redevelopment help a community understand how to better address their own problems and avoid the same issues. The first case study is an exemplary suburban commercial corridor redevelopment plan. The second case study presents how an organization has addressed issues of parking and circulation. The final case study is a state development manual for best management building practices.



4.1 West Main Street Corridor Plan - City of Sun Prairie, Wisconsin

In August of 2006, the City of Sun Prairie, Wisconsin adopted the West Main Street Corridor Plan (the Plan) for the redevelopment of their main commercial corridor. Highway 151 is a major arterial in the city and is dotted with big box retail, outdated commercial stripmalls and automobile oriented office and retail buildings.

Prior to the plan, Sun Prairie had seen many large developments surrounding the Main Street Corridor which caused the City to realize it needed to modernize its older commercial neighborhoods. The West Main Street Corridor Plan was created as a tool to create a singular and integrated design plan for the future redevelopment of the entire corridor.

The focus of the Plan is to “create a central hub, repositioned to serve new mixed-use developments as well as existing city neighborhoods.” By ridding the corridor of current forms such as one-story buildings and large parking lots and replacing them with two or three-story structures, mixed-uses and improved parking and pedestrian access. The Plan hopes to create a more dense and more urban street character. The Plan focuses on the following principles:

- “Place buildings near the street edge to help define a consistent ‘street façade’”
- “Discourage large parking lots in front of the buildings; place parking lots to the rear or side”
- “Locate active uses, such as retail and sidewalk cafes, along pedestrian routes”
- “Create guidelines to encourage coordinated development and quality architectural character”

Design guidelines for the corridor are also included in the Plan for building layout, architectural design, and pedestrian accessibility. Uses activating the street, as recommended in the Plan, should be established in the corridor as a means to increase economic and sidewalk activity. As stated in the plan, “people want to walk by buildings or places where they can look in and see something interesting or something that entices them inside.”

The City of New Berlin is facing many of the same issues Sun Prairie faced before establishing a redevelopment plan. The East National Avenue Plan seeks to solve the same issues of irregular setbacks, parking access and aesthetics. The objectives for the East National Avenue Plan are modeled after Sun Prairie’s plan, including the consistency objective which seeks to achieve a uniform corridor.

4.2 Victorian Transport Policy Institute's Parking Management Study

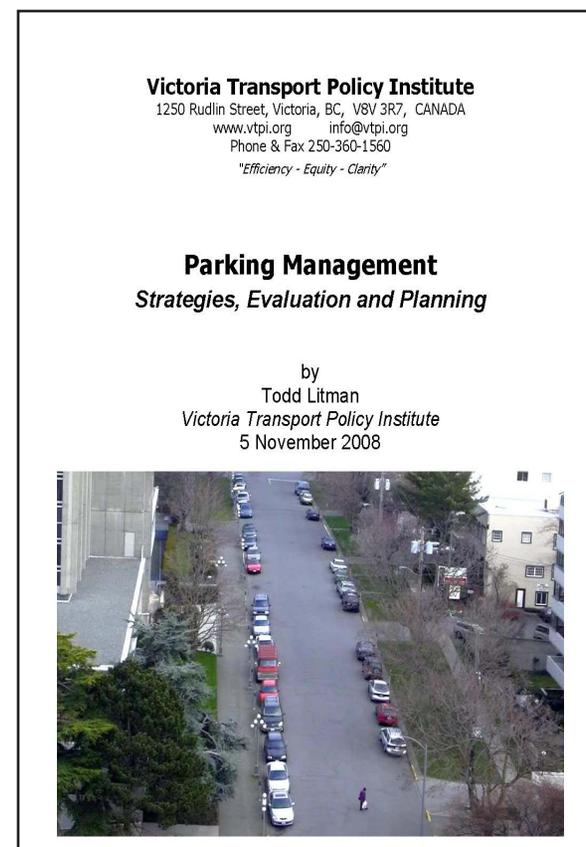
The Victorian Transport Policy Institute published a report, *Parking Management: Strategies, Evaluation and Planning* in 2008.¹ This report highlights ways conventional parking requirements are excessive and “waste resources, both directly, by increasing the money and land devoted to parking facilities and indirectly, by increasing automobile use and sprawl” (Litman 9). One example for determining parking requirements is to use contingency-based planning, which advises using lower parking standards over average or maximum parking standards. Parking demand is monitored after parking lots are constructed and, if more parking is actually needed, a contingency strategy is employed to address this need. New Berlin Code of Ordinances regulates the City's parking management with conventional over supply. Contingency strategies which can be useful in New Berlin are reflected in the table below.

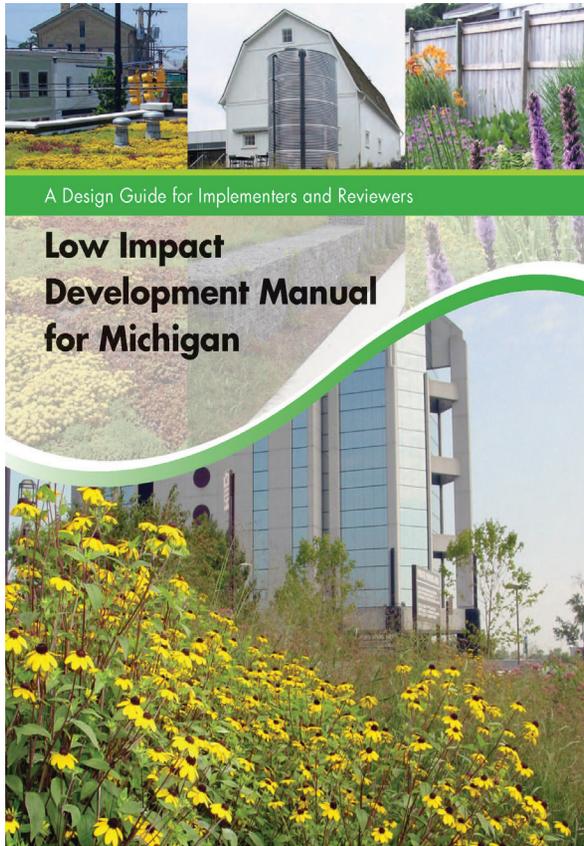
Some of the parking management benefits discussed by the Victorian Transport Policy Institute include facility cost savings, flexible facility location and design, revenue generation, reduced land consumption, mobility management, smart growth, improved walkability, more livable communities, as well as reduced stormwater management costs, water pollution and heat island effects. The parking management report recommends, “parking requirements should reflect each particular situation and should be flexibly” (Litman 7).

Parking Management Strategies that can work for the City of New Berlin

Strategy	Description	Typical Reduction
Shared Parking	Parking spaces serve multiple users and destinations.	10-30%
More Accurate and Flexible Standards	Adjust parking standards to more accurately reflect demand in a particular situation.	10-30%
Parking Maximums	Establish maximum parking standards.	10-30%
Walking and Cycling Improvements	Improve walking and cycling conditions to expand the range of destinations serviced by a parking facility.	5-15%
Bicycle Facilities	Proved bicycle storage and changing facilities.	5-15%

¹ Parking Management - Strategies, Evaluation and Planning by Todd Litman Victoria Transport Policy Institute. 5 November 2008





4.3 Low Impact Development Manual for Michigan

This 2008 manual is the cornerstone of Michigan’s Low Impact Development (LID) program. LID is the name given to the process of stormwater management in land development. The manual provides a comprehensive source for agencies, builders, communities and developers on current and best practices for new development and redevelopment projects. The scope of the document spans from the community level down to individual sites with detailed technical illustrations and content on stormwater management at all levels.

The LID manual defines these processes and principles to maximize a successful water runoff management project:

- Plan first
- Prevent, then mitigate
- Minimize disturbance
- Manage stormwater as a resource — not a waste
- Mimic the natural water cycle
- Disconnect. Decentralize. Distribute
- Integrate natural systems
- Maximize the multiple benefits of LID
- Use LID everywhere
- Make maintenance a priority

The following key points selected from specific chapters of Michigan’s LID Plan are incorporated into the East National Avenue Corridor Regulating Plan Sustainability Guidelines.

Key points taken from Chapter Four, *Integrating LID at the Community Level*:

- Ensure the purposed parking standards provide effective management of stormwater runoff from vehicle areas
- Include maximum parking regulations to prevent excessive parking
- Develop parking standards that reflect average parking needs rather than highest possible needs
- Allow for permeable material use to allow water infiltration
- Encourage the use of native plants in landscaping requirements
- Encourage cluster development (i.e., conservation subdivisions) as a method for preserving natural areas and reducing impervious surfaces
- Allow for downspouts to connect to vegetated areas on a property, not directly to a storm sewer

Key points taken from Chapter Five, *Incorporating LID into the Site Design Process*:

- Develop initial concept design using nonstructural best management practices (BMP) such as woodland and wetland protection, clustering and minimizing impervious surfaces
- Apply a blend of structural and nonstructural BMP that achieve a specific site's stormwater needs
- Protect natural flow of water pathways
- Reduce impervious surfaces and stormwater disconnection

Key points taken from Chapter Six, *Structural Best Management Practices*:

- Develop a matrix of appropriate structural BMPs for different site conditions
- Structural BMP selection is influenced by aesthetic/habitat related issues, applicability by land use, runoff quantity and runoff quality needs, maximization of dual use, cost and site factors
- A non-exhaustive list of possible structural BMPs:
 - Bioretention (rain gardens)
 - Capture Reuse (cisterns, rain barrels)
 - Constructed Filter along roadway
 - Detention Basins (ponds)
 - Pervious Pavement with Infiltration
 - Vegetated Swale (or bioswale)
 - Vegetated Roof

5

The Regulating Plan

D 5.1 The Development Code

L 5.2 Landscaping Guidelines

S 5.3 Sustainability Guidelines

N 5.4 Node Designs

D

5.1 The Development Code

5.1.1 City Center Zone

5.1.2 Suburban Commercial Zone

5.1.3 Western Gateway Zone

The Regulating Plan for East National Avenue

The East National Avenue Regulating Plan is composed of three sections—the development code, the landscaping guidelines and the sustainable practices guidelines.

The code presented in each chapter is location-based. Each property in the corridor is designated with a color. The designated color corresponds with a set of regulating codes. These codes regulate buildings, parking and pedestrian access, whereas the chapters on landscaping and sustainability guidelines are general recommendations for all properties throughout the corridor.

Each chart in the regulation section has four headings: Development Feature, Required, Optimal and Further Information. The Development Feature column lists the feature being regulated. The required column denotes the requirements all buildings must adhere to in the color-based location. The Optimal column provides additional options for property owners to go above and beyond the required regulations but are optional. The final column, Further Information, provides additional information to a property owner or developer.

Each zone has a distinct character which differentiates it from other areas of the corridor and the following regulations reflect these differences.



The City Center Zone

This zone is the main focus for development and redevelopment in the City of New Berlin. It is the densest area of the corridor and the center of commercial activity. This zone is designated with the color **green**.



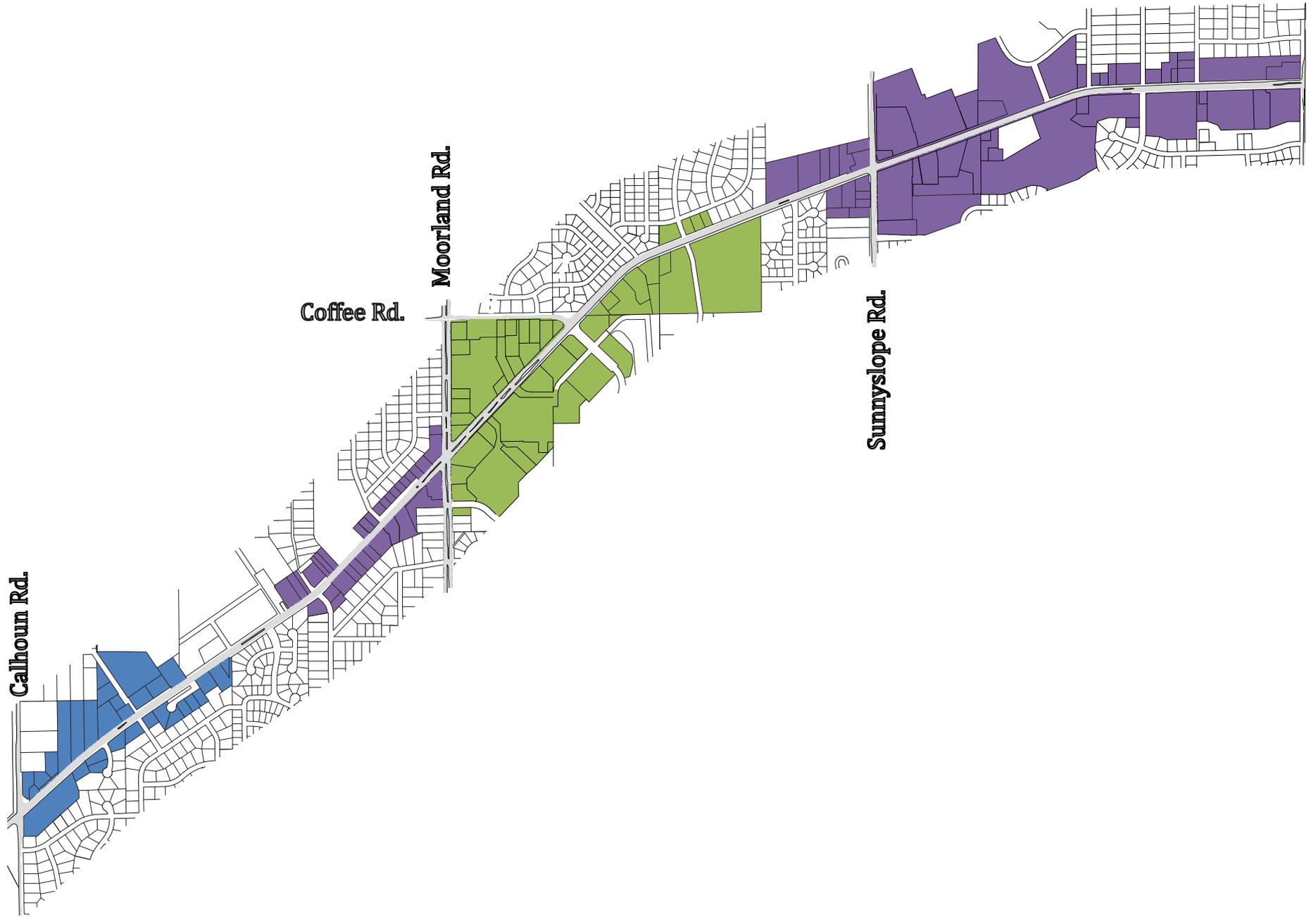
The Suburban Commercial Zone

This zone is the secondary commercial area of the corridor. It is less dense than the City Center Zone and serves as a transition into low density commercial areas or residential neighborhoods of New Berlin. This zone is designated with the color **purple**.



The Western Gateway Zone

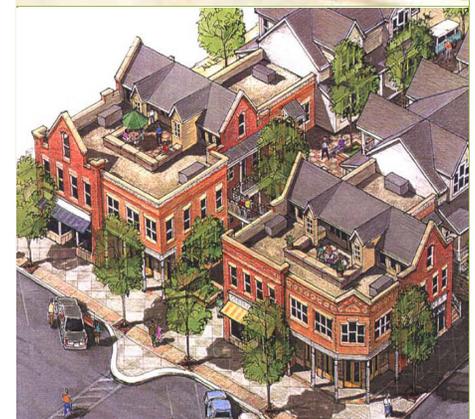
This zone is the least dense in the corridor. It includes a mix of rural commercial business and single-family homes. It serves as the gateway between the rural, western half of the City of New Berlin and the denser, eastern half. This zone is designated with the color **blue**.



B

Buildings

City Center Zone



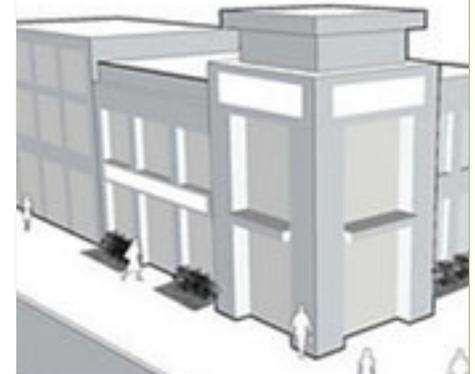
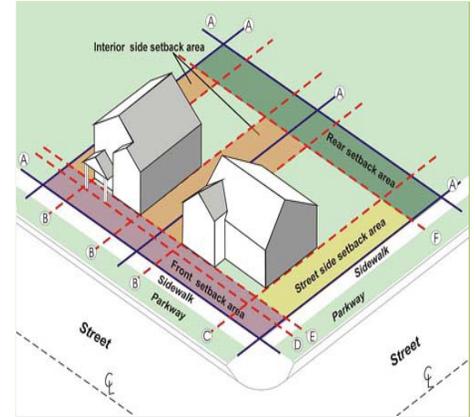
Development Feature	Required	Optimal	Specifications and Further Information
<p>Land Use - Commercial</p> <p>The purpose for which the land is being used.</p>	<p>Building use must follow future land use designation as found in Chapter 10 of the 2020 New Berlin Comprehensive Plan.</p>	<p>Mixed -uses containing commercial, residential and office space uses on one parcel is encouraged.</p>	<p>N/A</p>
<p>Land Use - Residential</p> <p>The purpose for which the land is being used.</p>	<p>Building use must follow future land use designation as found in Chapter 10 of the 2020 New Berlin Comprehensive Plan.</p>	<p>Mixed-uses, with office space or retail on the ground floor and residential above is highly encouraged</p>	<p>N/A</p>
<p>Setback - Commercial</p> <p>The separation between the right-of-way line and a building or structure.</p>	<p>Buildings facing National Avenue Maximum set back of 10-15 feet from the right-of-way Buildings Facing Moorland Road Maximum set back of 10-15 feet from the right-of-way Buildings Facing Coffee Road Maximum set back of 20-25 feet from the right-of-way</p>	<p>N/A</p>	<p>The façade of the building facing the street must be parallel to the edge of the street.</p>



B

Buildings

City Center Zone



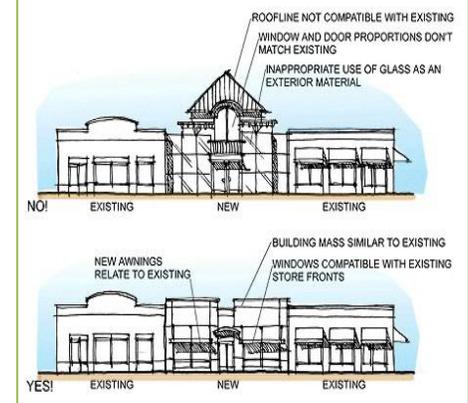
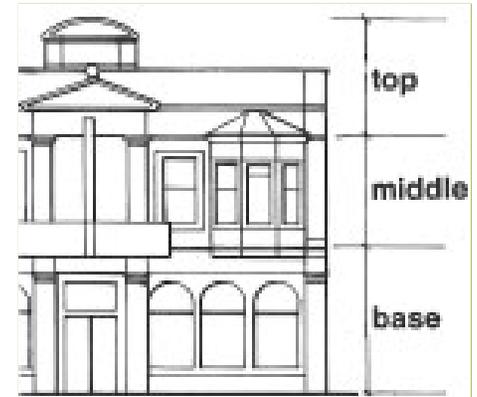
Development Feature	Required	Optimal	Specifications and Further Information
<p>Setback - Residential</p> <p>The separation between the right-of-way line and a building or structure.</p>	<p>Buildings facing National Avenue</p> <ul style="list-style-type: none"> • Maximum set back of 10-15 feet from the right-of-way <p>Buildings Facing Moorland Road</p> <ul style="list-style-type: none"> • Maximum set back of 10-15 feet from the right-of-way <p>Buildings Facing Coffee Road</p> <ul style="list-style-type: none"> • Maximum set back of 20-25 feet from the right-of-way 	<p>N/A</p>	<p>The façade of the building facing the street must be parallel to the edge of the street.</p>
<p>Building Height - Commercial</p> <p>The maximum height a building can be built.</p>	<p>The minimum height for any building is two-stories.</p>	<p>The minimum height for any building is two-stories.</p>	<p>No building may have a false façade to meet building height requirements.</p>
<p>Building Height - Residential</p> <p>The maximum height a building can be built.</p>	<p>The minimum height for any building is two-stories.</p>	<p>Buildings may reach a maximum height of four-stories.</p>	<p>No building may have a false façade to meet building height requirements.</p>



B

Buildings

City Center Zone



Development Feature	Required	Optimal	Specifications and Further Information
<p>Building Composition - Commercial</p> <p>The manner in which the façade of the building is constructed to contain a distinct base, middle, and top (see illustration).</p>	<p>All buildings must be composed of base, middle, and top elements.</p>	<p>N/A</p>	<p>The base of the building is the ground floor. Its facade should be well designed and highly attractive for pedestrians.</p> <p>The transition between the base and other levels of the building should be composed of contrasting materials, window openings, or ornamental elements.</p> <p>The top of the building provides an interesting silhouette.</p>
<p>Building Composition - Residential</p> <p>The manner in which the façade of the building is constructed to contain a distinct base, middle, and top (see illustration).</p>	<p>All buildings must be composed of base, middle, and top elements.</p>	<p>N/A</p>	<p>The base of the building is the ground floor. Its facade should be well designed and highly attractive for pedestrians.</p> <p>The transition between the base and other levels of the building should be composed of contrasting materials, window openings, or ornamental elements.</p> <p>The top of the building provides an interesting silhouette.</p>
<p>Materials – Commercial</p> <p>The products used to construct the façade of the building.</p>	<p>All buildings must follow City of New Berlin Design Guidelines as found in Chapter 275, Art. 8, Sect 275-59 of the New Berlin Code.</p>	<p>The building exceeds the requirements for use of masonry on building facades as found in Chapter 275, Art. 8, Sect 275-59 of the New Berlin Code.</p>	<p>N/A</p>



B

Buildings

City Center Zone



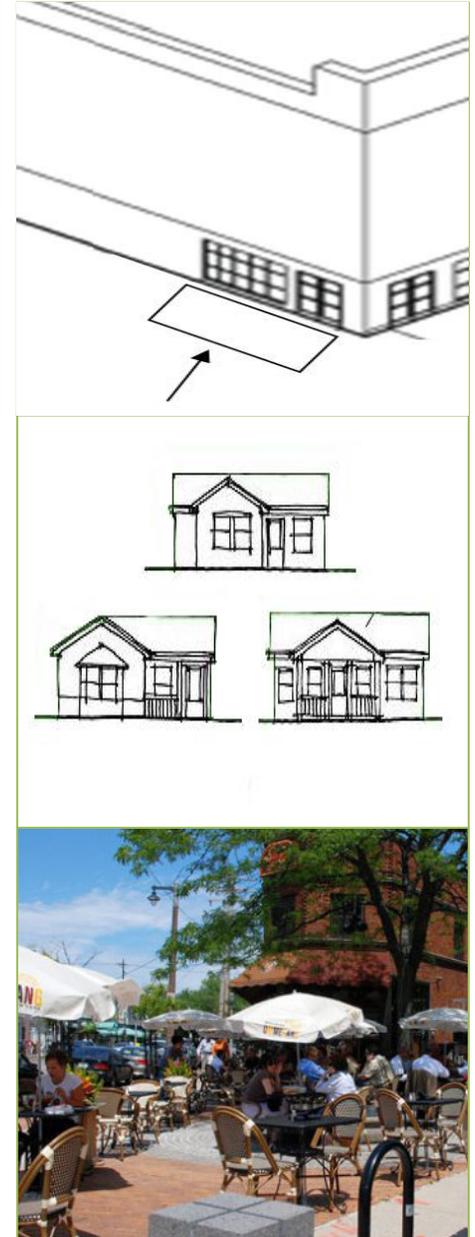
Development Feature	Required	Optimal	Specifications and Further Information
<p>Materials – Residential</p> <p>The products used to construct the façade of the building.</p>	<p>All buildings must follow City of New Berlin Design Guidelines as found in Chapter 275, Art. 8, Sect 275-59 of the New Berlin Code.</p>	<p>The building exceeds the requirements for use of masonry on building facades as found in Chapter 275, Art. 8, Sect 275-59 of the New Berlin Code.</p>	<p>N/A</p>
<p>Pedestrian Entrances - Commercial</p> <p>The main entrance to the building.</p>	<p>All buildings must have at least one entrance facing the main street, which is easily identified and emphasized through the use of architectural details or special materials. All buildings with parking to the rear of the building must include at least one entrance facing the main street in addition to any rear entrances.</p>	<p>N/A</p>	<p>A side entrance may be allowed if it is highly visible to pedestrians from the main street through the use of architectural details or special materials.</p>
<p>Pedestrian Entrances - Residential</p> <p>The main entrance to the building.</p>	<p>All buildings must have at least one entrance which is easily identified and emphasized through the use of architectural details or special materials.</p>	<p>The main entrance should face the main street.</p>	<p>Mixed-use buildings must have separate entrances for residents and business</p>



B

Buildings

City Center Zone



Development Feature	Required	Optimal	Specifications and Further Information
<p>Street Level Windows - Commercial</p> <p>Windows located at the base of the building, facing the sidewalk or main street.</p>	<p>All buildings must have 50% of street facing facades covered in clear window glazing between 2-8 feet above grade.</p>	<p>Additional glazing is desired.</p>	<p>N/A</p>
<p>Street Level Windows - Residential</p> <p>Windows located at the base of the building, facing the sidewalk or main street.</p>	<p>All buildings must adhere to New Berlin Building Codes regarding the provision of windows in residential buildings.</p>	<p>Additional glazing is desired.</p>	<p>N/A</p>
<p>Gathering Spaces - Commercial</p> <p>Building features that support community gathering and social interaction.</p>	<p>The Building must include outdoor gathering spaces such as plazas, seating areas, or areas for outdoor dining.</p>	<p>N/A</p>	<p>Gathering space must be visible from the public right-of-way.</p>



B

Buildings

City Center Zone

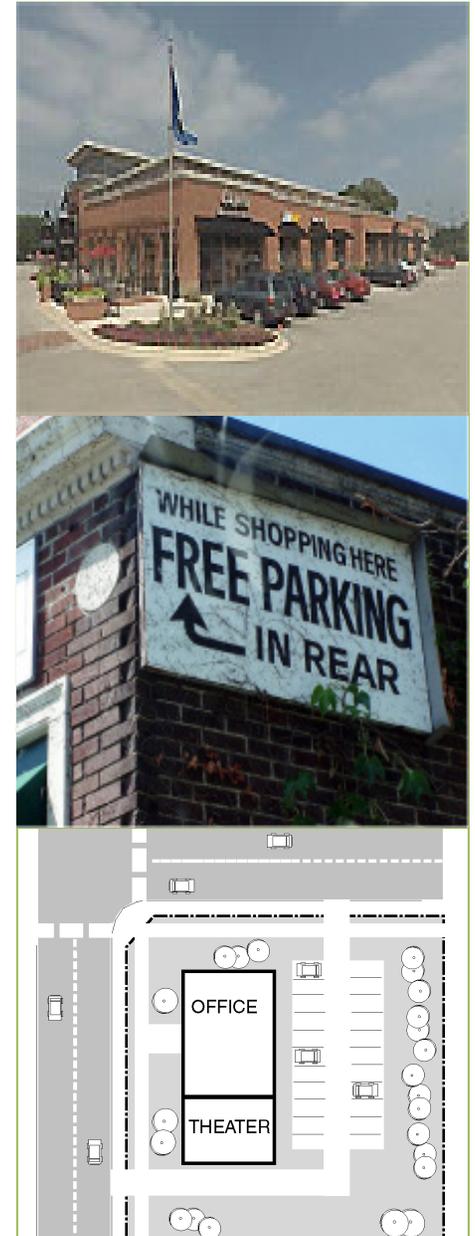


Development Feature	Required	Optimal	Specifications and Further Information
<p>Gathering Spaces - Residential</p> <p>Building features that support community gathering and social interaction.</p>	<p>The Building must include gathering spaces such as plazas or seating areas.</p>	<p>Buildings should include balconies or porches to encourage outdoor gathering.</p>	<p>N/A</p>



Parking

City Center Zone



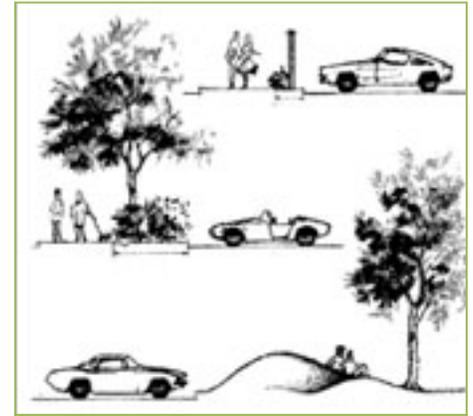
Development Feature	Required	Optimal	Specifications and Further Information
<p>Location</p> <p>The location of parking spaces within the parcel.</p>	<p>All parking must be located on the side or rear of the building. No parking spaces will be permitted in the front yard.</p>	<p>Parking is located to the rear of the building with little or no side parking.</p> <p>OR</p> <p>All parking is underground.</p>	<p>N/A</p>
<p>Parking Spaces</p> <p>The number of required parking spaces.</p>	<p>The maximum spaces allowed will equal 25% less than City of New Berlin parking regulations require for the specific used.</p>	<p>The property owner will conduct a parking study to determine the exact amount of spaces required for business operation.</p>	<p>N/A</p>
<p>Shared Parking</p> <p>The shared use of adjoining parking lots.</p>	<p>When feasible, parking areas should be shared by adjacent users and mixed-use developments to eliminate unnecessary parking stalls.</p>	<p>Adjoining parking lots should be combined in all instances in order to reduce unneeded spaces.</p>	<p>Parking lots can be shared between adjacent commercial, institutional, and residential uses if it can be demonstrated that the respective users have differing peak time parking demands.</p> <p>See Section A.2</p>



P

Parking

City Center Zone



Development Feature	Required	Optimal	Specifications and Further Information
<p>Parking Screen</p> <p>A buffer that creates a transition or separation between parking lots and other buildings and/or the street.</p>	<p>Any parking lot that abuts an adjacent property, sidewalk, or main street must provide screening through the use of decorative walls, fencing, plantings or any combination of the three.</p>	<p>N/A</p>	<p>The maximum height of any decorative wall or fencing is 3 feet.</p> <p>Walls and fencing must be built with high quality materials. Chain-link fencing is prohibited.</p> <p>Shrubs, bushes and grasses must not exceed 3 feet in height.</p>



P

Pedestrians

City Center Zone



Development Feature	Required	Optimal	Specifications and Further Information
<p>Access</p> <p>The right of entry for pedestrians.</p>	<p>Buildings must make onsite connections to off-site pedestrian paths and parking areas.</p>	<p>When possible, connections should be made to nearby recreation trails.</p>	<p>N/A</p>
<p>Circulation</p> <p>The design of pedestrian side paths.</p>	<p>Onsite sidepaths must be designed so that they are universally accessible and clearly defined.</p> <p>Any development must continue sidepath expansion along the major street.</p>	<p>Pedestrian seating, such as decorative benches, should be provided.</p>	<p>N/A</p>
<p>Bike Racks</p> <p>The required number of bike racks.</p>	<p>One bicycle space per twenty-five vehicle parking spaces must be provided. A minimum of one bicycle space must be provided for each building.</p>	<p>More than one bicycle space per twenty-five vehicle parking spaces should be provided.</p>	<p>N/A</p>



B

Buildings

Suburban Commercial Zone



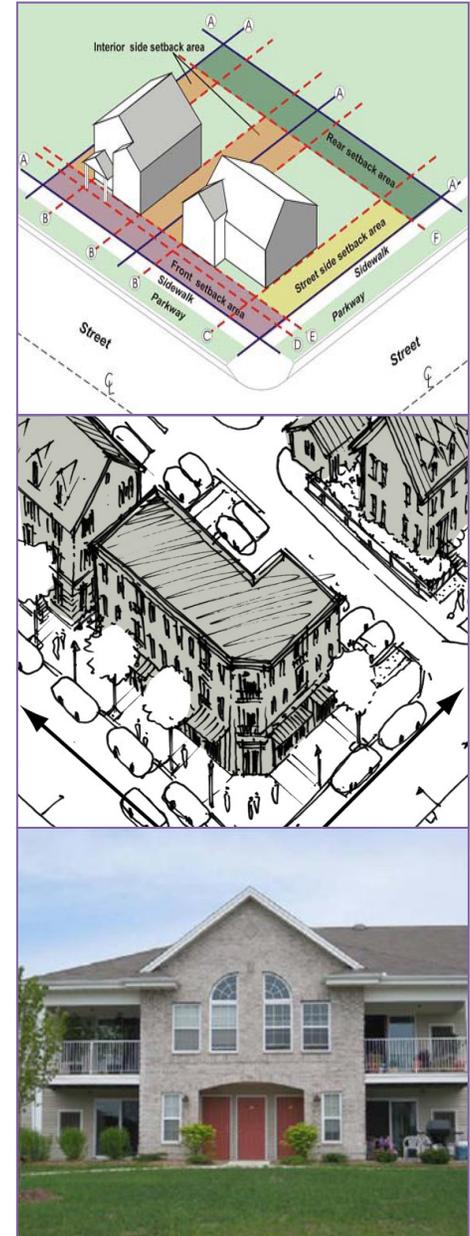
Development Feature	Required	Optimal	Specifications and Further Information
<p>Land Use - Commercial</p> <p>The purpose for which the land is being used.</p>	<p>Building use must follow future land use designation as found in Chapter 10 of the 2020 New Berlin Comprehensive Plan.</p>	<p>Mixed -uses containing commercial, residential and office space uses on one parcel is encouraged.</p>	<p>N/A</p>
<p>Land Use - Residential</p> <p>The purpose for which the land is being used.</p>	<p>Building use must follow future land use designation as found in Chapter 10 of the 2020 New Berlin Comprehensive Plan.</p>	<p>Mixed-uses, with office space or retail on the ground floor and residential above is highly encouraged.</p>	<p>N/A</p>
<p>Setback - Commercial</p> <p>The separation between the right-of-way line and a building or structure.</p>	<p>Buildings facing National Avenue</p> <ul style="list-style-type: none"> • Maximum set back of 15-20 feet from the right-of-way <p>Buildings Facing Moorland Road</p> <ul style="list-style-type: none"> • Maximum set back of 15-20 feet from the right-of-way <p>Buildings Facing a Side Street</p> <ul style="list-style-type: none"> • Maximum set back of 20-25 feet from the right-of-way 	<p>N/A</p>	<p>The façade of the building facing the street must be parallel to the edge of the street.</p>



B

Buildings

Suburban Commercial Zone



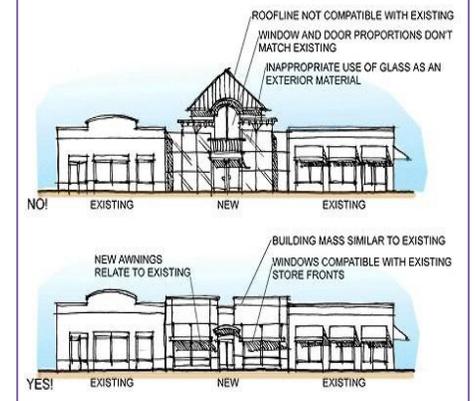
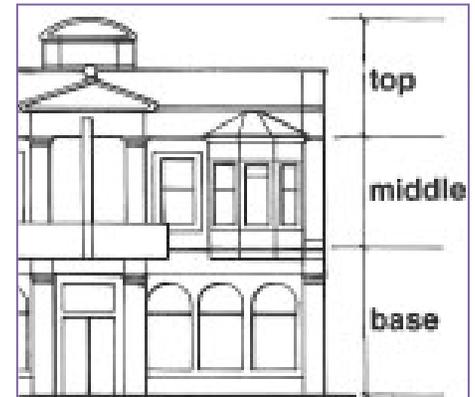
Development Feature	Required	Optimal	Specifications and Further Information
<p>Setback - Residential</p> <p>The separation between the right-of-way line and a building or structure.</p>	<p>Buildings facing National Avenue</p> <ul style="list-style-type: none"> Maximum set back of 15-20 feet from the right-of-way <p>Buildings Facing Moorland Road</p> <ul style="list-style-type: none"> Maximum set back of 15-20 feet from the right-of-way <p>Buildings Facing Side Street</p> <ul style="list-style-type: none"> Maximum set back of 20-25 feet from the right-of-way 	<p>N/A</p>	<p>The façade of the building facing the street must be parallel to the edge of the street.</p>
<p>Building Height - Commercial</p> <p>The maximum height a building can be built.</p>	<p>The minimum height for any building is two-stories.</p>	<p>Buildings may reach a maximum height of three-stories.</p>	<p>No building may have a false façade to meet building height requirements.</p>
<p>Building Height - Residential</p> <p>The maximum height a building can be built.</p>	<p>The minimum height for any building is two-stories.</p>	<p>Buildings may reach a maximum height of three-stories.</p>	<p>No building may have a false façade to meet building height requirements.</p>



B

Buildings

Suburban Commercial Zone



Development Feature	Required	Optimal	Specifications and Further Information
<p>Building Composition - Commercial</p> <p>The manner in which the façade of the building is constructed to contain a distinct base, middle, and top (see illustration).</p>	<p>All buildings must be composed of base, middle, and top elements.</p>	<p>N/A</p>	<p>The base of the building is the ground floor. Its facade should be well designed and highly attractive for pedestrians.</p> <p>The transition between the base and other levels of the building should be composed of contrasting materials, window openings, or ornamental elements.</p> <p>The top of the building provides an interesting silhouette.</p>
<p>Building Composition - Residential</p> <p>The manner in which the façade of the building is constructed to contain a distinct base, middle, and top (see illustration).</p>	<p>All buildings must be composed of base, middle, and top elements.</p>	<p>N/A</p>	<p>The base of the building is the ground floor. Its facade should be well designed and highly attractive for pedestrians.</p> <p>The transition between the base and other levels of the building should be composed of contrasting materials, window openings, or ornamental elements.</p> <p>The top of the building provides an interesting silhouette.</p>
<p>Materials – Commercial</p> <p>The products used to construct the façade of the building.</p>	<p>All buildings must follow City of New Berlin Design Guidelines as found in Chapter 275, Art. 8, Sect 275-59 of the New Berlin Code.</p>	<p>The building exceeds the requirements for use of masonry on building facades as found in Chapter 275, Art. 8, Sect 275-59 of the New Berlin Code.</p>	<p>N/A</p>

B Buildings

Suburban Commercial Zone

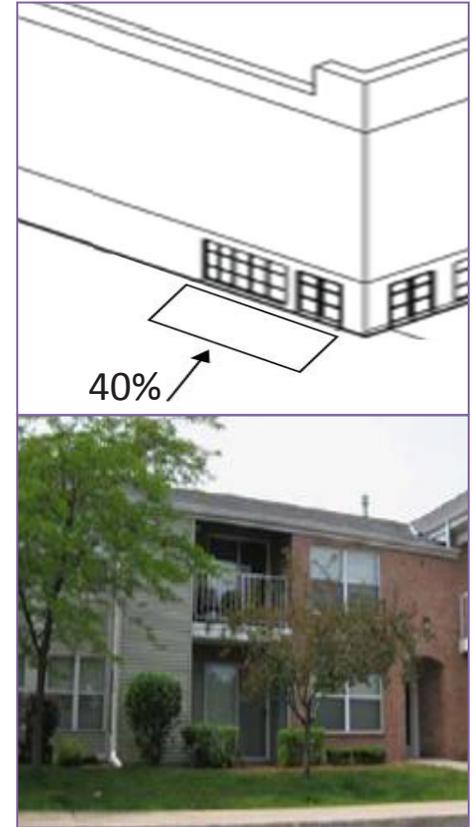


Development Feature	Required	Optimal	Specifications and Further Information
<p>Materials – Residential</p> <p>The products used to construct the façade of the building.</p>	<p>All buildings must follow City of New Berlin Design Guidelines as found in Chapter 275, Art. 8, Sect 275-59 of the New Berlin Code.</p>	<p>The building exceeds the requirements for use of masonry on building facades as found in Chapter 275, Art. 8, Sect 275-59 of the New Berlin Code.</p>	<p>N/A</p>
<p>Pedestrian Entrances - Commercial</p> <p>The main entrance to the building.</p>	<p>All buildings must have at least one entrance facing the main street, which is easily identified and emphasized through the use of architectural details or special materials. All buildings with parking to the rear of the building must include at least one entrance facing the main street in addition to any rear entrances.</p>	<p>N/A</p>	<p>A side entrance may be allowed if it is highly visible to pedestrians from the main street through the use of architectural details or special materials.</p>
<p>Pedestrian Entrances - Residential</p> <p>The main entrance to the building.</p>	<p>All buildings must have at least one entrance which is easily identified and emphasized through the use of architectural details or special materials.</p>	<p>The main entrance should face the main street.</p>	<p>Mixed-use buildings must have separate entrances for residents and business.</p>



B Buildings

Suburban Commercial Zone



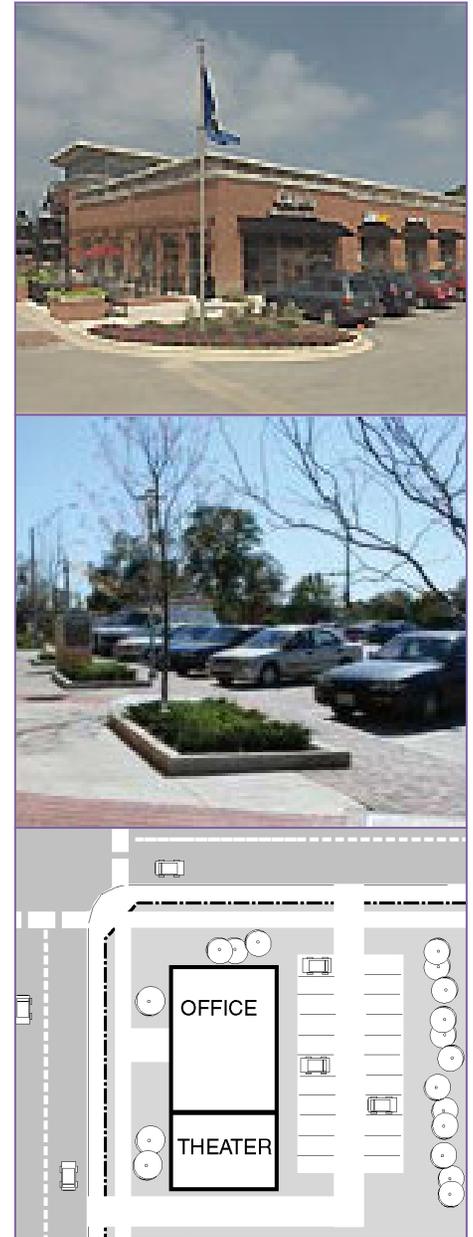
Development Feature	Required	Optimal	Specifications and Further Information
<p>Street Level Windows - Commercial</p> <p>Windows located at the base of the building, facing the sidewalk or main street.</p>	<p>All buildings must have 40% of street facing facades covered in clear window glazing between 2-8 feet above grade.</p>	<p>Additional glazing is desired.</p>	<p>N/A</p>
<p>Street Level Windows - Residential</p> <p>Windows located at the base of the building, facing the sidewalk or main street.</p>	<p>All buildings must adhere to New Berlin Building Codes regarding the provision of windows in residential buildings.</p>	<p>Additional glazing is desired.</p>	<p>N/A</p>



P

Parking

Suburban Commercial Zone



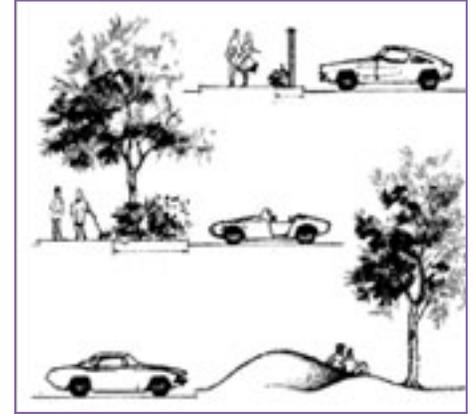
Development Feature	Required	Optimal	Specifications and Further Information
<p>Location</p> <p>The location of parking spaces within the parcel.</p>	<p>All parking must be located on the side or rear of the building. No parking spaces will be permitted in the front yard.</p>	<p>Parking is located to the rear of the building with little or no side parking.</p>	<p>N/A</p>
<p>Parking Spaces</p> <p>The number of required parking spaces.</p>	<p>The maximum spaces allowed will equal 25% less than City of New Berlin parking regulations require for the specific used.</p>	<p>The property owner will conduct a parking study to determine the exact amount of spaces required for business operation.</p>	<p>N/A</p>
<p>Shared Parking</p> <p>The shared use of adjoining parking lots.</p>	<p>When feasible, parking areas should be shared by adjacent users and mixed-use developments to eliminate unnecessary parking stalls.</p>	<p>Adjoining parking lots should be combined in all instances in order to reduce unneeded spaces.</p>	<p>Parking lots can be shared between adjacent commercial, institutional, and residential uses if it can be demonstrated that the respective users have differing peak time parking demands.</p> <p>See Section A.2</p>



P

Parking

Suburban Commercial Zone



Development Feature	Required	Optimal	Specifications and Further Information
<p>Parking Screen</p> <p>A buffer that creates a transition or separation between parking lots and other buildings and/or the street.</p>	<p>Any parking lot that abuts an adjacent property, sidewalk, or main street must provide screening through the use of decorative walls, fencing, plantings or any combination of the three.</p>	<p>N/A</p>	<p>The maximum height of any decorative wall or fencing is 3 feet.</p> <p>Walls and fencing must be built with high quality materials. Chain-link fencing is prohibited.</p> <p>Shrubs, bushes and grasses must not exceed 3 feet in height.</p>



P

Pedestrians

Suburban Commercial Zone



Development Feature	Required	Optimal	Specifications and Further Information
<p>Access</p> <p>The right of entry for pedestrians.</p>	<p>Buildings must make onsite connections to off-site pedestrian paths and parking areas.</p>	<p>When possible, connections should be made to nearby recreation trails.</p>	<p>N/A</p>
<p>Circulation</p> <p>The design of pedestrian side paths.</p>	<p>Onsite sidepaths must be designed so that they are universally accessible and clearly defined.</p> <p>Any development must continue sidepath expansion along the major street.</p>	<p>Pedestrian seating, such as decorative benches, should be provided.</p>	<p>N/A</p>
<p>Bike Racks</p> <p>The required number of bike racks.</p>	<p>One bicycle space per twenty-five vehicle parking spaces must be provided. A minimum of one bicycle space must be provided for each building.</p>	<p>More than one bicycle space per twenty-five vehicle parking spaces should be provided.</p>	<p>N/A</p>



P

Pedestrians

Suburban Commercial Zone



Development Feature	Required	Optimal	Specifications and Further Information
<p>Gathering Spaces - Commercial</p> <p>Building features that support community gathering and social interaction.</p>	N/A	The Building should include outdoor gathering spaces such as plazas, seating areas, or areas for outdoor dining.	Gathering space must be visible from the public right-of-way.
<p>Gathering Spaces - Residential</p> <p>Building features that support community gathering and social interaction.</p>	N/A	The Building should include outdoor gathering spaces such as plazas, seating areas, balconies or porches.	N/A



B

Buildings

Western Gateway Zone



Development Feature	Required	Optimal	Specifications and Further Information
<p>Land Use - Commercial</p> <p>The purpose for which the land is being used.</p>	<p>Building use must follow future land use designation as found in Chapter 10 of the 2020 New Berlin Comprehensive Plan.</p>	<p>Mixed -uses containing commercial, residential and office space uses on one parcel is encouraged.</p>	<p>N/A</p>
<p>Land Use - Residential</p> <p>The purpose for which the land is being used.</p>	<p>Building use must follow future land use designation as found in Chapter 10 of the 2020 New Berlin Comprehensive Plan.</p>	<p>Mixed-uses, with office space or retail on the ground floor and residential above is highly encouraged.</p>	<p>N/A</p>
<p>Setback - Commercial</p> <p>The separation between the right-of-way line and a building or structure.</p>	<p>Buildings facing National Avenue</p> <ul style="list-style-type: none"> • Maximum set back of 30-35 feet from the right-of-way <p>Buildings Facing Calhoun Road</p> <ul style="list-style-type: none"> • Maximum set back of 35-40 feet from the right-of-way <p>Buildings Facing a Side Street</p> <ul style="list-style-type: none"> • Maximum set back of 35-40 feet from the right-of-way 	<p>N/A</p>	<p>The façade of the building facing the street must be parallel to the edge of the street.</p>

B

Buildings

Western Gateway Zone



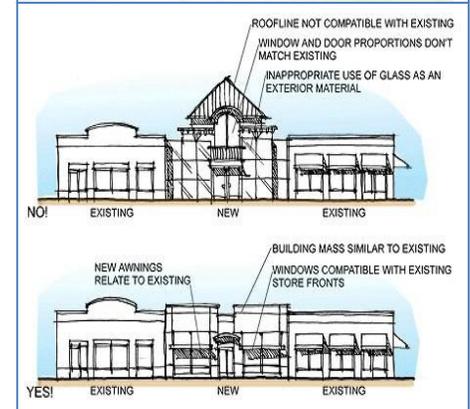
Development Feature	Required	Optimal	Specifications and Further Information
<p>Setback - Residential</p> <p>The separation between the right-of-way line and a building or structure.</p>	<p>Buildings facing National Avenue</p> <ul style="list-style-type: none"> Maximum set back of 30-35 feet from the right-of-way <p>Buildings Facing Calhoun Road</p> <ul style="list-style-type: none"> Maximum set back of 35-40 feet from the right-of-way <p>Buildings Facing a Side Street</p> <ul style="list-style-type: none"> Maximum set back of 35-40 feet from the right-of-way 	<p>N/A</p>	<p>The façade of the building facing the street must be parallel to the edge of the street.</p>
<p>Building Height - Commercial</p> <p>The maximum height a building can be built.</p>	<p>The minimum height for any building is one-story.</p>	<p>Buildings may reach a maximum height of two-stories.</p>	<p>No building may have a false façade to meet building height requirements.</p>
<p>Building Height - Residential</p> <p>The maximum height a building can be built.</p>	<p>The minimum height for any building is one-story.</p>	<p>Buildings may reach a maximum height of two-stories.</p>	<p>No building may have a false façade to meet building height requirements.</p>



B

Buildings

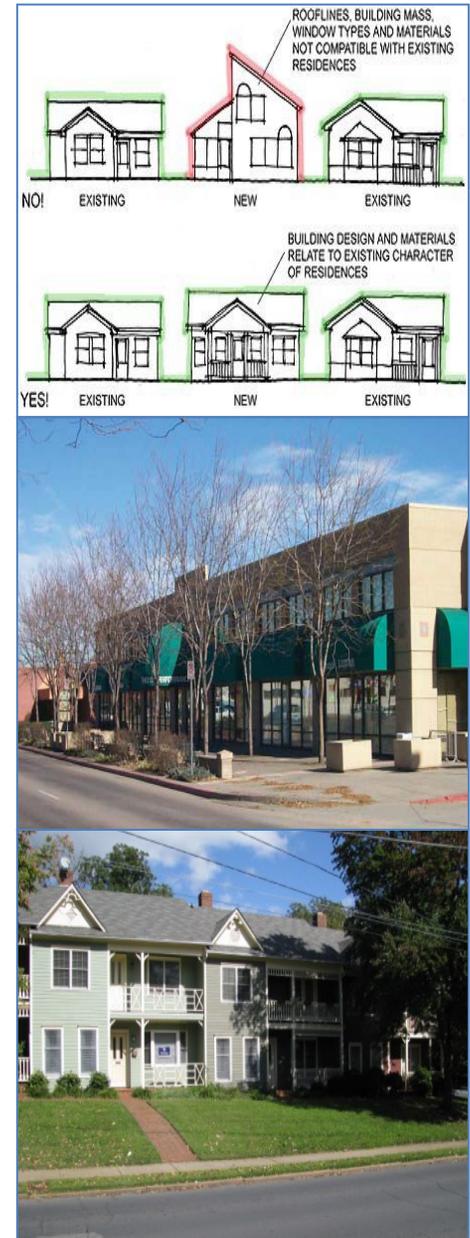
Western Gateway Zone



Development Feature	Required	Optimal	Specifications and Further Information
<p>Building Composition - Commercial</p> <p>The manner in which the façade of the building is constructed to contain a distinct base, middle, and top (see illustration).</p>	<p>All buildings must be composed of base, middle, and top elements.</p> <p>One story buildings are only required to be composed of base and top elements.</p>	<p>N/A</p>	<p>The base of the building is the ground floor. Its facade should be well designed and highly attractive for pedestrians.</p> <p>The transition between the base and other levels of the building should be composed of contrasting materials, window openings, or ornamental elements.</p> <p>The top of the building provides an interesting silhouette.</p>
<p>Building Composition - Residential</p> <p>The manner in which the façade of the building is constructed to contain a distinct base, middle, and top (see illustration).</p>	<p>All buildings must be composed of base, middle, and top elements.</p> <p>One story buildings are only required to be composed of base and top elements.</p>	<p>N/A</p>	<p>The base of the building is the ground floor. Its facade should be well designed and highly attractive for pedestrians.</p> <p>The transition between the base and other levels of the building should be composed of contrasting materials, window openings, or ornamental elements.</p> <p>The top of the building provides an interesting silhouette.</p>
<p>Materials – Commercial</p> <p>The products used to construct the façade of the building.</p>	<p>All buildings must follow City of New Berlin Design Guidelines as found in Chapter 275, Art. 8, Sect 275-59 of the New Berlin Code.</p>	<p>The building exceeds the requirements for use of masonry on building facades as found in Chapter 275, Art. 8, Sect 275-59 of the New Berlin Code.</p>	<p>N/A</p>

B Buildings

Western Gateway Zone



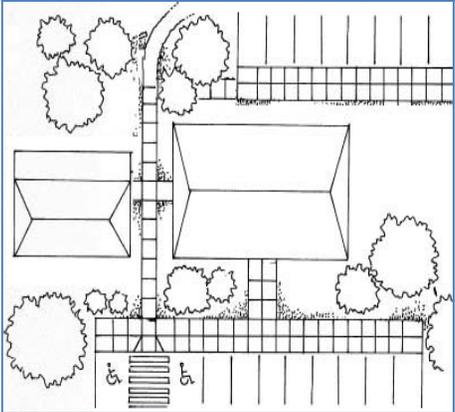
Development Feature	Required	Optimal	Specifications and Further Information
<p>Materials – Residential</p> <p>The products used to construct the façade of the building.</p>	<p>All buildings must follow City of New Berlin Design Guidelines as found in Chapter 275, Art. 8, Sect 275-59 of the New Berlin Code.</p>	<p>The building exceeds the requirements for use of masonry on building facades as found in Chapter 275, Art. 8, Sect 275-59 of the New Berlin Code.</p>	<p>N/A</p>
<p>Pedestrian Entrances - Commercial</p> <p>The main entrance to the building.</p>	<p>All buildings must have at least one entrance facing the main street, which is easily identified and emphasized through the use of architectural details or special materials. All buildings with parking to the rear of the building must include at least one entrance facing the main street in addition to any rear entrances.</p>	<p>N/A</p>	<p>A side entrance may be allowed if it is highly visible to pedestrians from the main street through the use of architectural details or special materials.</p>
<p>Pedestrian Entrances - Residential</p> <p>The main entrance to the building.</p>	<p>All buildings must have at least one entrance which is easily identified and emphasized through the use of architectural details or special materials.</p>	<p>The main entrance should face the main street.</p>	<p>N/A</p>



P

Parking

Western Gateway Zone

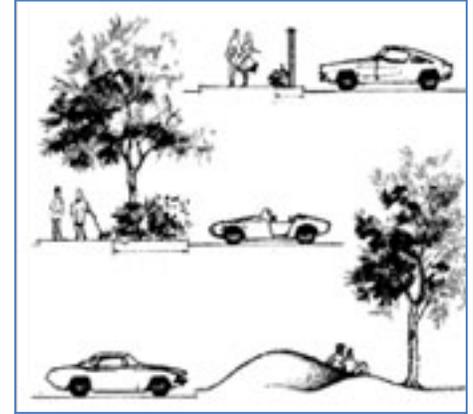


Development Feature	Required	Optimal	Specifications and Further Information
<p>Location</p> <p>The location of parking spaces within the parcel.</p>	<p>One row of parking can be located on the front yard of the building.</p> <p>The remaining parking must be located to the side or rear of the building.</p>	<p>No more than 50% of parking should be located in the front yard of the building.</p>	<p>N/A</p>
<p>Parking Spaces</p> <p>The number of required parking spaces.</p>	<p>The maximum spaces allowed will equal 25% less than City of New Berlin parking regulations require for the specific used.</p>	<p>The property owner will conduct a parking study to determine the exact amount of spaces required for business operation.</p>	<p>N/A</p>
<p>Shared Parking</p> <p>The shared use of adjoining parking lots.</p>	<p>When feasible, parking areas should be shared by adjacent users and mixed-use developments to eliminate unnecessary parking stalls.</p>	<p>Adjoining parking lots should be combined in all instances in order to reduce unneeded spaces.</p>	<p>Parking lots can be shared between adjacent commercial, institutional, and residential uses if it can be demonstrated that the respective users have differing peak time parking demands.</p> <p>See Section A.2</p>

P

Parking

Western Gateway Zone



Development Feature	Required	Optimal	Specifications and Further Information
<p>Parking Screen</p> <p>A buffer that creates a transition or separation between parking lots and other buildings and/or the street.</p>	<p>Any parking lot that abuts a an adjacent property, sidewalk, or main street must provide screening through the use of decorative walls, fencing, plantings or any combination of the three.</p>	<p>N/A</p>	<p>The maximum height of any decorative wall or fencing is 3 feet.</p> <p>Walls and fencing must be built with high quality materials. Chain-link fencing is prohibited.</p> <p>Shrubs, bushes and grasses must not exceed 3 feet in height.</p>



P

Pedestrians

Western Gateway Zone



Development Feature	Required	Optimal	Specifications and Further Information
<p>Access</p> <p>The right of entry for pedestrians.</p>	<p>Buildings must make onsite connections to off-site pedestrian paths and parking areas.</p>	<p>When possible, connections should be made to nearby recreation trails.</p>	<p>N/A</p>
<p>Circulation</p> <p>The design of pedestrian side paths.</p>	<p>Onsite sidepaths must be designed so that they are universally accessible and clearly defined.</p> <p>Any development must continue sidepath expansion along the Main Street.</p>	<p>Pedestrian seating, such as decorative benches, should be provided.</p>	<p>N/A</p>
<p>Bike Racks</p> <p>The required number of bike racks.</p>	<p>One bicycle space per twenty-five vehicle parking spaces must be provided. A minimum of one bicycle space must be provided for each building.</p>	<p>More than one bicycle space per twenty-five vehicle parking spaces should be provided.</p>	<p>N/A</p>



L

5.2 Landscaping Guidelines

The Regulating Plan for East National Avenue is composed of three sections—the development code, the landscaping guidelines, and the sustainable practices guidelines.

The Landscaping Guidelines include six general landscaping guidelines. They are meant to serve as a guide for new development and redevelopment in the corridor. The six guidelines are used to create a more uniform and attractive corridor. Having a vibrantly landscaped corridor will increase city beautification, create a cohesive identity and improve the long-term environmental sustainability of the corridor.

These guidelines should be used to evaluate proposed developments or redevelopments in the corridor. They are meant to be flexible and should be thought of as recommended procedures, rather than ridged policies. Enough flexibility is written into the guidelines for the City to use these guidelines as market conditions or community expectations change.

Landscaping adds an economic value to a property and helps increase surrounding property values. Beautiful plants and trees make a property more inviting and aesthetically pleasing. Plants and trees improve the air quality.

Plant materials shall be located to enhance views from public streets and sidepaths.

The placement of plants along sidepaths and streets helps to create a more walkable and friendly environment.



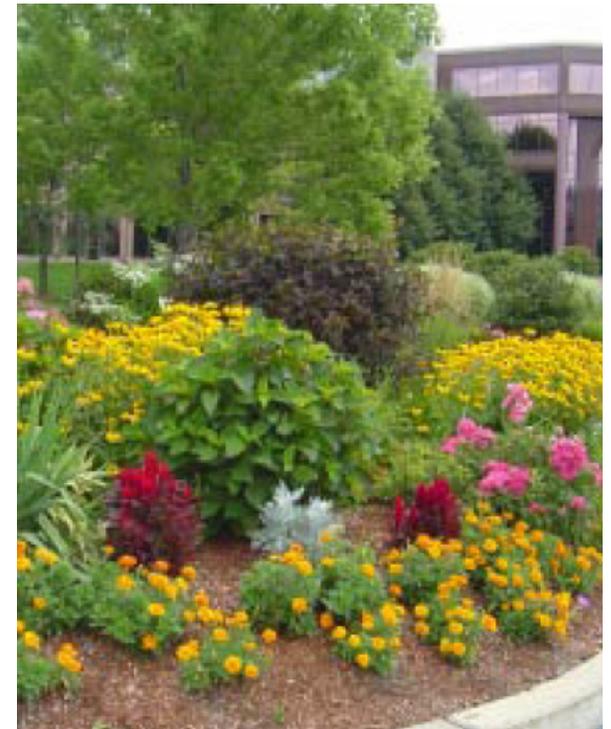
Shading of parking spaces should be maximized through the selection and arrangement of plants and trees.

This guideline is meant to encourage green parking lots. Shading of parking lots helps to decrease the amount of solar radiation that reaches the pavement. Shading helps decrease energy costs, greenhouse gas emissions and heat-related illnesses. Shading helps cool the pavement surface, extending its lifespan.



Plant materials shall be arranged to minimize safety hazards such as obstruction of access points on and off-site.

This guideline is intended to protect the safety of vehicles and pedestrians. Creating a clear entryway into a parking lot will allow for safe travel and the reduction of injury or harm to the general public.



Plants in perimeter landscaping areas should be grouped instead of evenly spaced.

Grouping of similar plant types helps to reduce costs and overall maintenance needed for landscaping.



Plant and tree arrangements should form a pattern of larger materials (trees) along the rear and smaller materials (shrubs and perennials) along the front.

Establishing patterns of healthy, hardy plants and trees helps to sustain landscaping.



Plant and tree choices should be functional and allow for stormwater management practices.

Stormwater management practices use vegetation for water treatment and management. Bioswales shown remove silt and pollution from runoff water.





5.3 Sustainability Guidelines

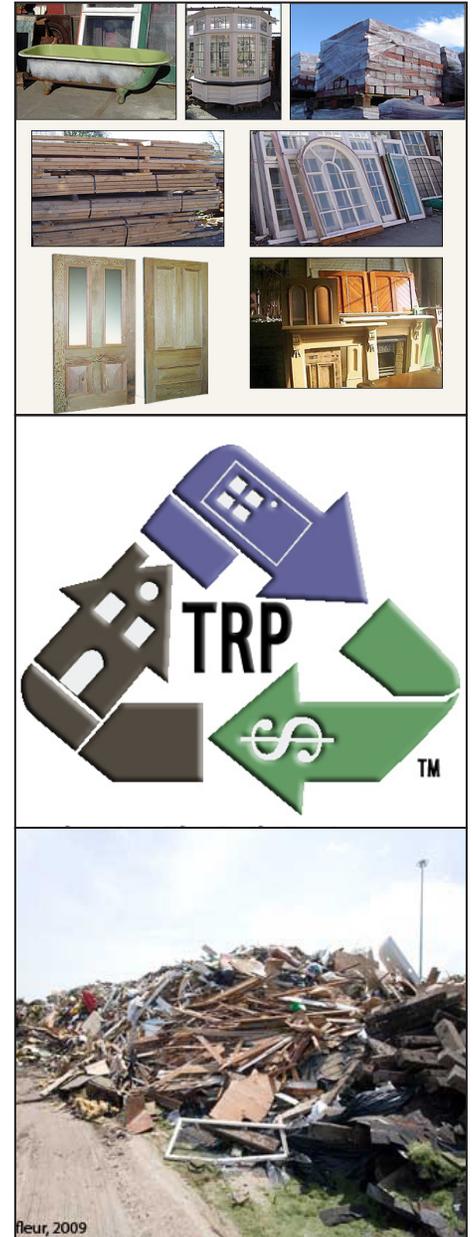
The Regulating Plan for East National Avenue is composed of three sections—the development code, the landscaping guidelines, and the sustainable practices guidelines.

The Sustainability Guidelines section includes general guidelines for water efficiency and sustainable building practices. The guidelines are written in order to promote building and building material reuse, energy efficiency and diverse stormwater management techniques. Incorporating these practices into the development or redevelopment of buildings in the corridor improves the long-term environmental and economic sustainability of the corridor.

These guidelines should be used to evaluate proposed developments or redevelopments in the corridor. They are meant to be flexible and should be thought of as recommended procedures rather than ridged policies. Enough flexibility is written into the guidelines for the City to use these guidelines as market conditions or community expectations change.

S

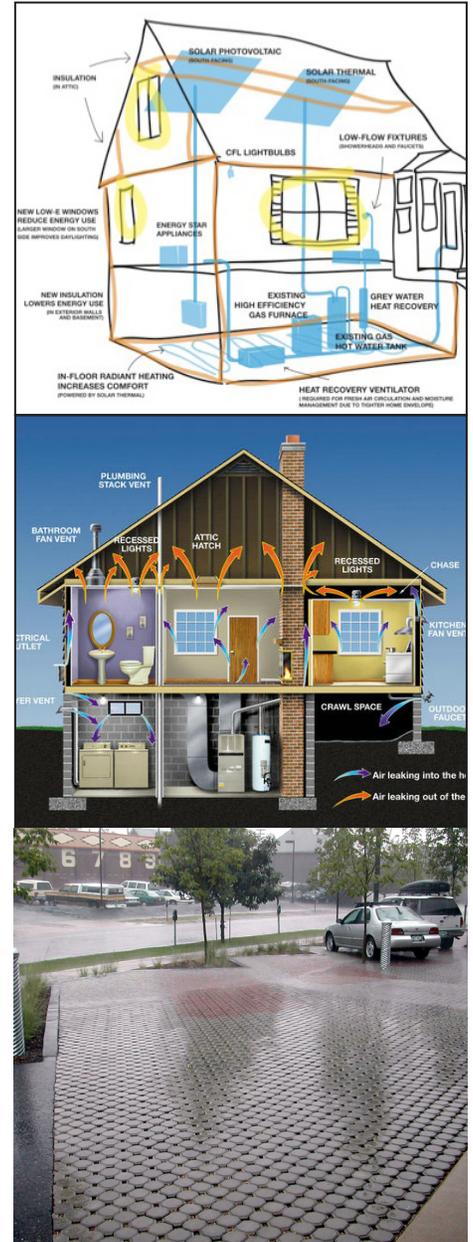
Sustainable Practices



Practice	CCZ	SCZ	WGZ	Goal	Specifications and Further Information
<p>Reuse of Building Materials</p> <p>Reduce demand for new materials and reduce waste going to landfills</p>	Yes	Yes	Yes	Ensure that at least 5% of a project's materials (based on value) comprise salvaged, refurbished or reused materials	BMRA (Best practices)
<p>Use of Recycled Materials</p> <p>Reduce demand for new materials and increase market for recycled materials</p>	Yes	Yes	Yes	Ensure that at least 15% of a project's construction materials (based on value) are comprised of recycled content	Green Home Green (local locator for recycled materials and contacts)
<p>Construction and Demolition Waste Management</p> <p>Reduce waste going to landfill</p>	Yes	Yes	Yes	Ensure that at least 75% of non-hazardous construction and demolition debris is recycled	BMRA (Best practices), Green Builder, Seach Directory

S

Sustainable Practices



Practice	CCZ	SCZ	WGZ	Goal	Specifications and Further Information
On-Site Renewable Resources Energy which comes from natural resources such as sunlight, wind, rain and geothermal heat, which are renewable (naturally replenished)	Yes	Yes	Yes	Buildings over 10,000 square feet should create 10% of their energy on-site from renewable resources	Customer-Owned Renewable Generation in Wisconsin, Renewable Energy in LEED™ Projects (Generation Options)
Energy Efficient Buildings Buildings that are environmentally responsible and resource-efficient throughout a building's life-cycle: from siting to design, construction, operation, maintenance, renovation and deconstruction	Yes	Yes	Yes	Buildings over 10,000 square feet should conform, at a minimum, to the requirements of the LEED Green Building Rating System at the Silver performance level or other equivalent U.S. green building standards	Green Building Pages, Green Building Alliance (Education-Materials), U.S. Green Building Council

Building Materials Reuse Association (BMRA), <http://www.bmra.org/best-practices>

Home Green Home, A Wisconsin recycle buiding organization, <http://www.homegreenhome.biz/>

Green Building Pages, *Sorce green materials and information for developers and contractors*, <http://www.greenbuildingpages.com/>

Green Builder, Directory, <http://directory.greenbuilder.com/search.gbpro>

Customer-Owned Renewable Generation in Wisconsin, Source of on-site power generation from Wisconsin Public Service, http://www.wisconsinpublicservice.com/environment/generation_wi.aspx

Renewable Energy in LEED™ Projects, http://www.focusonenergy.com/files/document_management_system/renewables/renewableleedprojects_factsheet.pdf

Green Building Alliance, Education on green building, <http://www.wgba.org/education.html>

U.S. Green Building Council, Source of green building materials, <http://www.usgbc.org>

W

Water Efficiency



Development Feature	CCZ	SCZ	WGZ	Application	Installation Cost	Specifications and Further Information
<p>Bio-Swales</p> <p>Shallow stormwater channels that capture and infiltrate runoff and can also remove pollutants</p>	Yes	Yes	Yes	Alternative to conventional curb and gutter conveyance systems	<p>*Construction Cost (per linear foot) \$4.50 - \$8.50 (from seed)</p> <p>\$15 - \$20 (from sod)</p>	<p>*LID Manual for Michigan (P. 325) MMSD - FRESH COAST GREEN SOLUTIONS (p. 13)</p>
<p>Constructed Filter</p> <p>Structures or excavated areas containing a layer of filtration media that reduce pollutant levels in stormwater runoff</p>	Yes	Limited	No	Suitable for sites without sufficient surface area available for bio-retention	*\$10,000 (case study in <i>City of Wayne, MI</i>)	*LID Manual for Michigan (p. 158)
<p>Green Roofs</p> <p>Roofs that are partially or completely covered with vegetation allowing roofs to function more like a vegetated surface</p>	Yes	Yes	Limited	When other water runoff solutions are unavailable or insufficient	<p>*\$5.60/ft for extensive roofs</p> <p>\$15/ft for intensive roofs plus cost of any structural reinforcement</p>	* Philadelphia Stormwater Manual v1.5 - Green Roofs 6.2.1 (p. 15)

W

Water

Efficiency

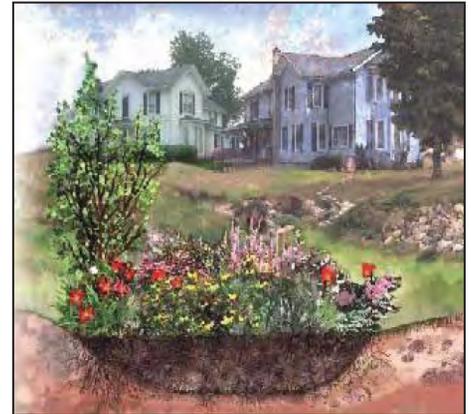


Development Feature	CCZ	SCZ	WGZ	Application	Installation Cost	Specifications and Further Information
<p>Greenways</p> <p>Vegetated strips that help to infiltrate and evaporate rainwater and snow melt</p>	No	No	Yes	Can be placed along bike paths, sidewalks, riverbanks and streets	*\$38 - \$95 per linear foot	*MMSD - FRESH COAST GREEN SOLUTIONS (p. 16)
<p>Porous Pavement</p> <p>Paving that allows water to filter to the soil below</p>	Yes	Yes	Yes	The ideal location for porous pavement is in low traffic or overflow parking areas	\$2 - \$4 per square foot	* Philadelphia Stormwater Manual v1.5 - Porous Pavement 6.2.1 (p. 27)
<p>Rainwater Collection (Rain barrels/cisterns)</p> <p>Structures designed to catch and store runoff from rooftops to allow for its reuse</p>	Yes	Yes	Yes	When collected water can be used for other uses and other runoff solutions are unavailable	<p>* Rain barrel = \$150/each</p> <p>Cistern = \$1,000 (500 gallon) to \$5,000 (6,500 gallon underground)</p>	* STORMWATER RUNOFF REDUCTION PROGRAM INTERIM REPORT (p. 72)

W

Water

Efficiency



Development Feature	CCZ	SCZ	WGZ	Application	Installation Cost	Specifications and Further Information
Rain Gardens Gardens watered by collected or pooled stormwater runoff, slowly infiltrating it into the ground along root pathways	Yes	Yes	Yes	Place near downspouts on buildings when room for landscaping is available	\$5 to \$10 per square foot *Bioretention areas cost approximately \$5-7 per cubic foot of storage	*LID Manual for Michigan (p. 145)
Stormwater Trees Trees particularly suited for absorbing rainfall	Yes	Yes	Yes	Long-range water management solution Requires mature trees for peak efficiency	* \$200 - \$340/tree	* STORMWATER RUNOFF REDUCTION PROGRAM INTERIM REPORT (p. 7)

LID Manual for Michigan

<http://library.semcog.org/InmagicGenie/DocumentFolder/LIDManualWeb.pdf>

MMSD - FRESH COAST GREEN SOLUTIONS

<http://v3.mmsd.com/AssetsClient/Documents/sustainability/SustainBookletweb1209.pdf>

Philadelphia Stormwater Manual v1.5 - Green Roofs 6.2.1

http://www.indygov.org/eGov/City/DPW/Old%20CMS%20Docs/PIO/DRAFT_SW%20Green%20Doc_Section%204.pdf

STORMWATER RUNOFF REDUCTION PROGRAM INTERIM REPORT

http://v3.mmsd.com/AssetsClient/Documents/waterqualityresearch/stormwater_runoff_reduction_program_interim_report.pdf

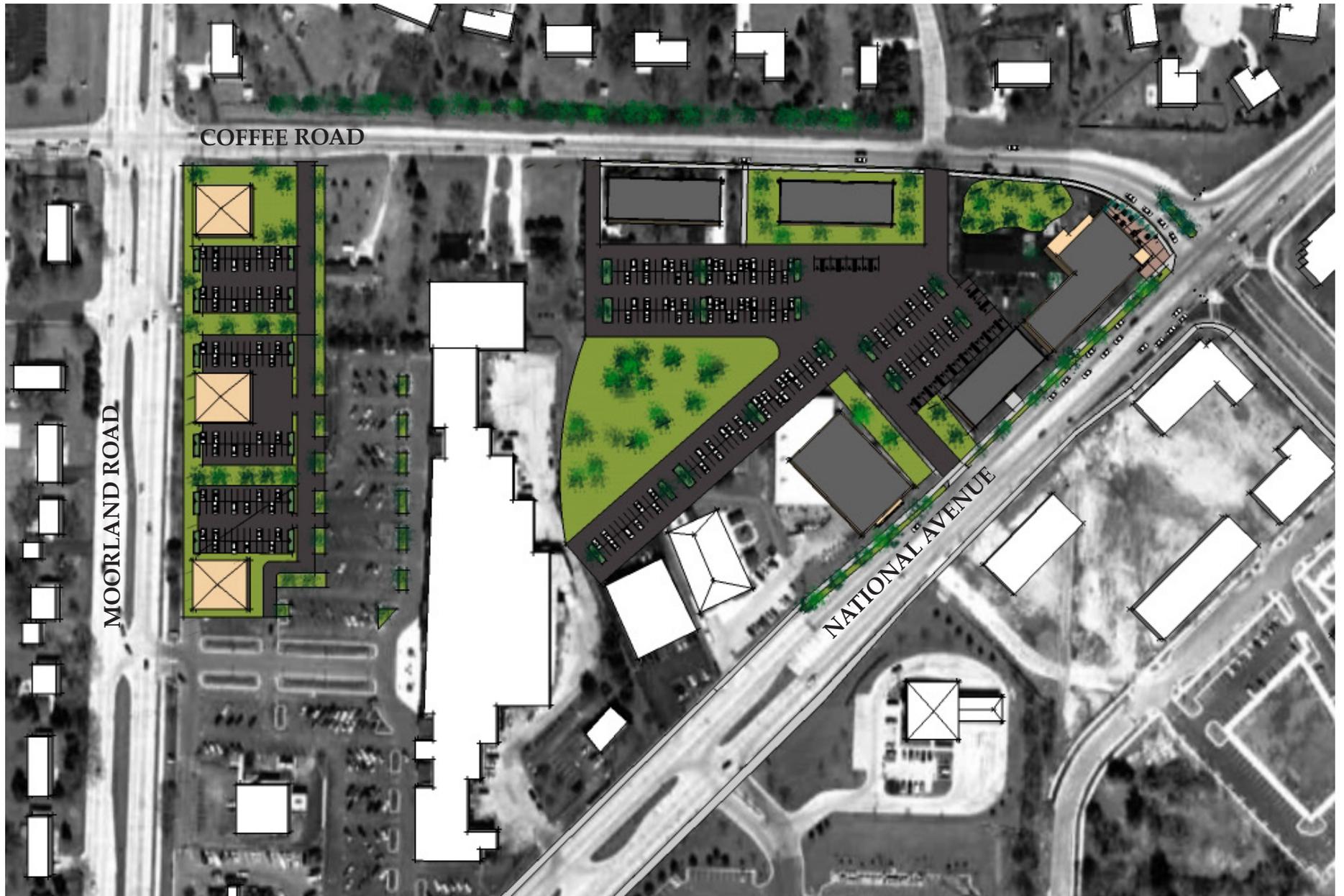
N

5.4 Node Designs

5.4.1 Coffee Road Node

5.4.2 Sunnyslope at National Avenue Node

5.4.3 Western Gateway Node

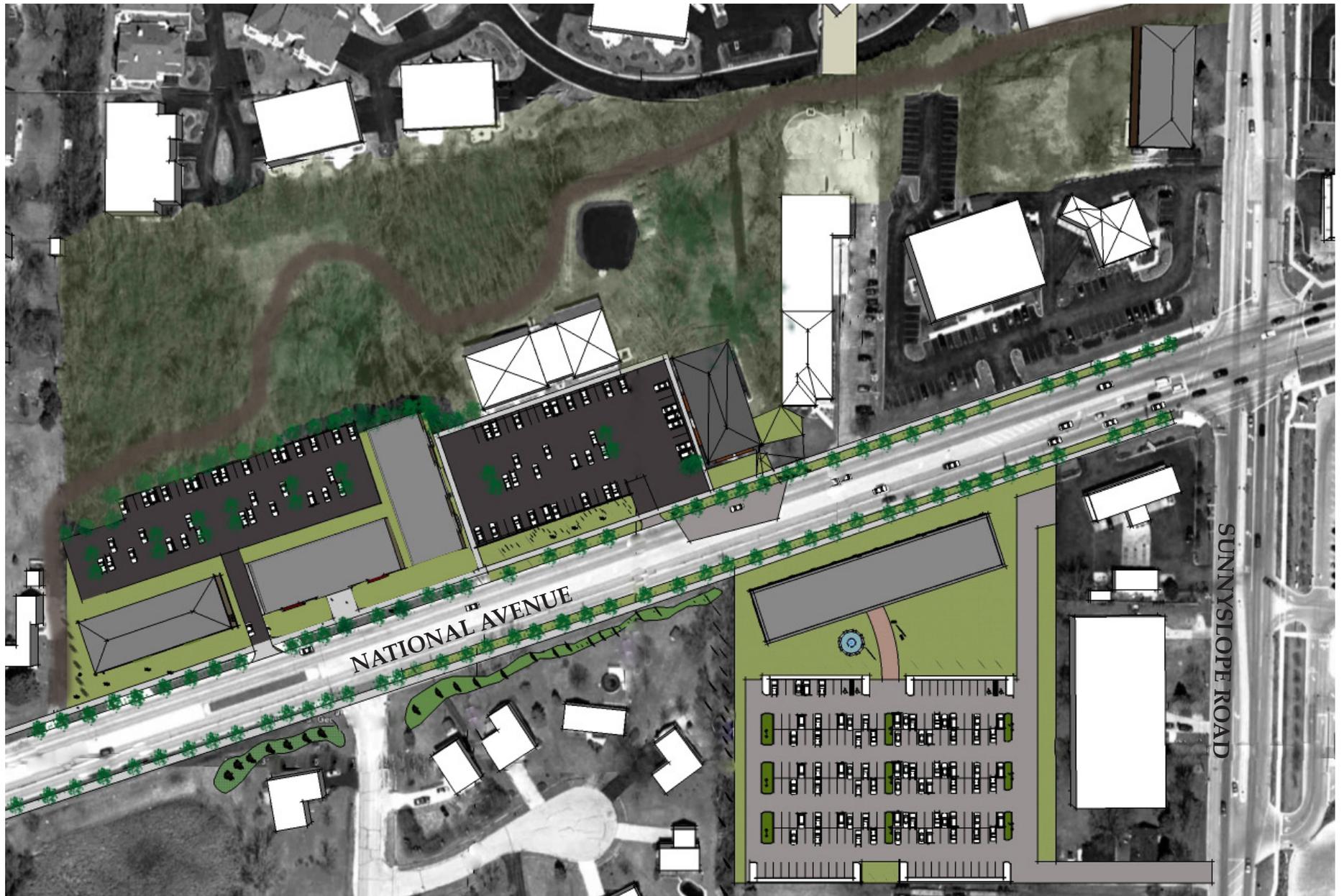


Conceptual designs for future development build-out condition are offered to illustrate the development regulations and guidelines for the Corridor. These designs will aid City staff when approached for redevelopment of the corridor.

5.4.1 Coffee Road Node Design

The Coffee Road node offers new commercial buildings along Coffee Road as well as National Avenue. New commercial buildings on Coffee Road match the future land use for Coffee Road. The form and scale of buildings will mimic that of the City Center development on the south side of National Avenue. Buildings are brought parallel to the street edge and conform to the new setback regulations. This design offers a public plaza at the northwest corner of Coffee Road and National Avenue. Comments received at the open house (discussed in Chapter 3) were positive in regards to this plaza. Citizens indicated an interest in public gathering places.





5.4.2 Sunnyslope at National Avenue Node Design

The Sunnyslope at National Avenue node design encompasses complementary retail along National Avenue, primarily west of Sunnyslope Road. Retail buildings address the street edge of National Avenue, with parking placed along the side and rear of the new buildings.

This design also includes a new commercial building located at the southwest corner of Sunnyslope Road and National Avenue, which addresses the street and conforms to the new regulations. Future off-site trail connections are proposed which connect Sunnyslope Road and National Avenue.





5.4.3 Western Gateway Node Design

The Western Gateway Node Design maintains residential land uses and allows for small commercial development. An option for a senior community is placed to the rear of the parcel. Small retail stores or commercial businesses line National Avenue and are brought parallel to the street. In this node, one row of parking is allowed in front of the commercial buildings, while maintaining the largest setback along National Avenue.



6

Recommendations for Implementation



1 General implementation shall begin with the adoption of the East National Avenue Corridor Regulating Plan through resolution of the City of New Berlin Plan Commission.

2 The East National Avenue Corridor Regulating Plan's regulations and guidelines shall be implemented over time through market driven development and redevelopment of the corridor. Future actions taken by developers, property owners and City staff should reflect the regulations and guidelines detailed within the East National Avenue Corridor Regulating Plan. It is important for development and redevelopment in the near future to act as a precedent, setting the bar high for future development and redevelopment projects in the corridor.

3 The City of New Berlin should implement a new Planned Unit Development (PUD) policy specifically targeted for the East National Avenue Corridor. It is recommended the City establish minimum size for a PUD to be one half an acre (0.5 acres).

The purpose of the Planned Development District is to encourage desirable development, redevelopment, rehabilitation and conservation in the City of New Berlin by allowing for greater flexibility. Planned Unit Developments at this size should only be allowed if not found to be harmful to the environment, property values or character of the surrounding neighborhood. Furthermore, only PUDs which are found to be in accordance with the spirit of the City of New Berlin Comprehensive Plan and the East National Avenue Corridor Plan should be allowed.

Implementing a small size PUD is reasonable for the City of New Berlin. At the current requirement of five acres, PUD developments are not feasible in much of the East National Avenue Corridor. Other communities in the greater Milwaukee area have successfully implemented small PUD regulations. The Village of Shorewood in Milwaukee County has established a minimum PUD size of one half of one acre (0.5 acres) in order to redevelop small infill parcels. Although Shorewood and New Berlin differ greatly in terms of density, both communities face similar situations such as small parcels and little room for new, large scale redevelopment. The Village of Shorewood has been successful in creating small PUD parcels in order to implement the visions set forth in their Comprehensive Plan.

Planned Unit Developments allow city planning staff to be more flexible with project developers. In the current City Code, developers must adhere to every standard. In a PUD development, city staff can recommend the Plan Commission allow developers to bypass certain requirements in exchange for something more desirable. For example, if a developer provides more greenspace in their proposal than what city code requires, planning staff could recommend the Plan Commission allow increased density on the site.

Furthermore, this ability to negotiate with developers is an easy way for the city to incorporate sustainable practices into corridor developments. Because these practices are not mandated by city code, developers are under no obligation to incorporate sustainable technology into their developments. PUDs provide a way for the City to encourage the use of sustainable practices such as bioswales, impervious pavements, or rain gardens. In exchange, the City can relax some code requirements such as parking or greenspace requirements, so long as they do not adversely affect the City's public health, safety, or general welfare.

4 The City of New Berlin should increase opportunities for pedestrian movement and circulation in the corridor.

One of the major issues discussed by city residents at the open house was the lack of available pedestrian crossings in the corridor. There are particular cases, especially in the Western Gateway area, where large areas of residential housing are cut off from community parks and civic buildings because of the wide roadway. The City should further examine the corridor in search of possible areas to expand pedestrian crossings in order to increase community connectivity.

Part of this examination should include the expansion of sidepaths along major streets into the City's residential areas. While the entire East National Avenue Corridor has sidepaths along both sides of the streets, many of these sidepaths do not extend into the neighboring housing developments. The lack of connectivity between uses discourages pedestrian movements and increases the corridor's dependence on automobile circulation. It is recommended the city expand sidepath expansion north and south along major streets crossing National Avenue such as Moorland Road and Sunnyslope Road. This will expand pedestrian access to surrounding residential neighborhoods.

The City should also plan for multiple forms of pedestrian transportation. One of the issues discovered is, at current pedestrian crossings, cross walk buttons on stop lights are not placed at an accessible height for disabled persons. This is one issue the City should address immediately, especially in areas with high amounts of senior housing. Bicycle transportation should also be planned for within the corridor. New developments should be encouraged to expand connections to bike trails and should provide bicycle parking in conjunction with vehicle parking. By adapting sidepaths for use by multiple users, the City of New Berlin can encourage pedestrian travel without dramatic changes to the current system.

5 The City of New Berlin should work with Waukesha County to incorporate pedestrian friendly improvements on National Avenue when the County decides to replace the roadway. Although there is no current plan for the County to replace the roadway, the City should be prepared to work with the County when the time comes.

The City can play an active role in the re-design of this vital corridor in the community. Residents indicated the lack of pedestrian crossings, inability to physically cross the street because of high traffic and the dangerous nature of National Avenue as major concerns in the corridor. By working directly with the County, the City can ensure these concerns are met.

There are various methods in which the City can help to achieve these needs. Pedestrian sidepath bump-outs can help to slow traffic and increase sidepath usage and safety. A second method to decrease traffic speeds is with center boulevards. Boulevards, such as that in the City of West Allis on National Avenue, help to reduce traffic speeds by narrowing the roadway. Additionally, boulevards create community greenspace which can help treat roadway stormwater run-off and give pedestrians a safe “resting point” when trying to cross the street. In particular, these roadway improvements should be sought in the City Center area where pedestrian movement is highly encouraged.

6 The City of New Berlin should continue to implement the street lighting scheme currently in place along sections of National Avenue. When fully completed, this lighting scheme will help to unify the different nodes along the corridor as well as create a sense of place for the City of New Berlin.

7 The City of New Berlin should create a Façade Improvement Grant program to encourage corridor redevelopment.

The National Avenue Business Survey indicated moderate support for façade improvement grants in the corridor. These grants can be a useful tool for the city in creating a unifying look for the corridor. Due to the corridor's age, some buildings look outdated and are in need of improvements. While the buildings may not be cohesive in design, businesses occupying these buildings are valuable to the community and the corridor. Façade improvement grants are one way to address this issue.

Façade improvement grants give current businesses the opportunity to improve the look of their building while minimizing financial hardship. Typically, the City agrees to match a certain amount of funds with the building owner for the improvements. In exchange for the grant, the building owner agrees to adhere to the City's design standards. By using this tool, the City can improve the overall look of the building character without massive redevelopment.

8 The City of New Berlin should encourage the creation of an East National Avenue Business Improvement District (BID) by business owners with information on the importance of BIDs and ways they can help local businesses.

According to the National Avenue Business Survey, 53% of business owners in the corridor indicated they either need additional information on BIDs or they might be interested in the formation of such an organization. The City of New Berlin should take the lead in providing adequate information to business owners in order to inform them of the typical operations of Business Improvement Districts, their benefits and drawbacks. By providing the necessary information to business owners, the City can help to encourage business owners to pool resources for the benefit of the corridor.

Business Improvement Districts help local businesses in many ways. BIDs can help fund certain improvements such as streetscaping or sidepath decorations. Successful BIDs have also provided marketing for district businesses. The City can also use BIDs as a funding source for grant or loan programs. Other BIDs across the State of Wisconsin have used BID funds to support façade improvement grant programs or small business start-up loan programs.

While local business owners will ultimately decide if a BID is feasible in the corridor, the City should support its creation as a way to reduce the financial burden of corridor improvements.

Parcel Consolidation

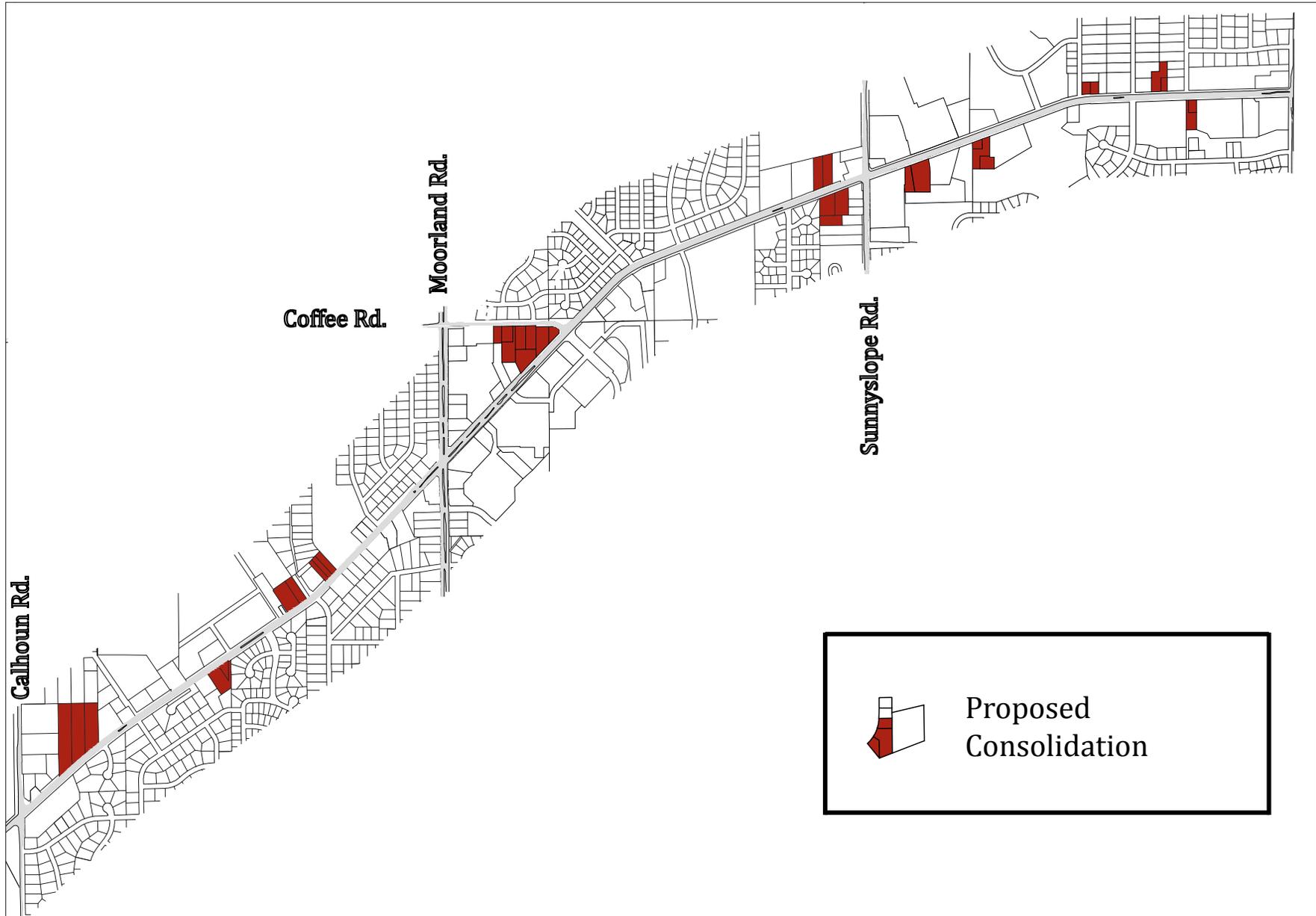
Modern retail and development standards require large parcels in order to meet building codes and standards, as well as maximize profit from development. This map identifies parcels within the East National Avenue Corridor which are ripe for consolidation, in order to accommodate development needs.

The parcels which are highlighted are prime candidates for future consolidation which will expand redevelopment opportunities. The characteristics which put these parcels on the map are the following:

- Parcels adjacent to priority redevelopment parcels (See Ch 2.7)
- Lot size which limits development opportunities
- Shape of current parcel is ill-suited for planned purpose
- Parcel is land-locked and would isolate the parcel from commercial traffic

Consolidating parcels also allows for a Planned Unit Development (PUD) to occur, should the City's allowable PUD size not be amended in the future.

Future Parcel Consolidation Opportunities



A Appendix

A.1 Priority Parcels

A.2 Curb Cuts and Parking

A.3 Business Survey

A.4 Open House

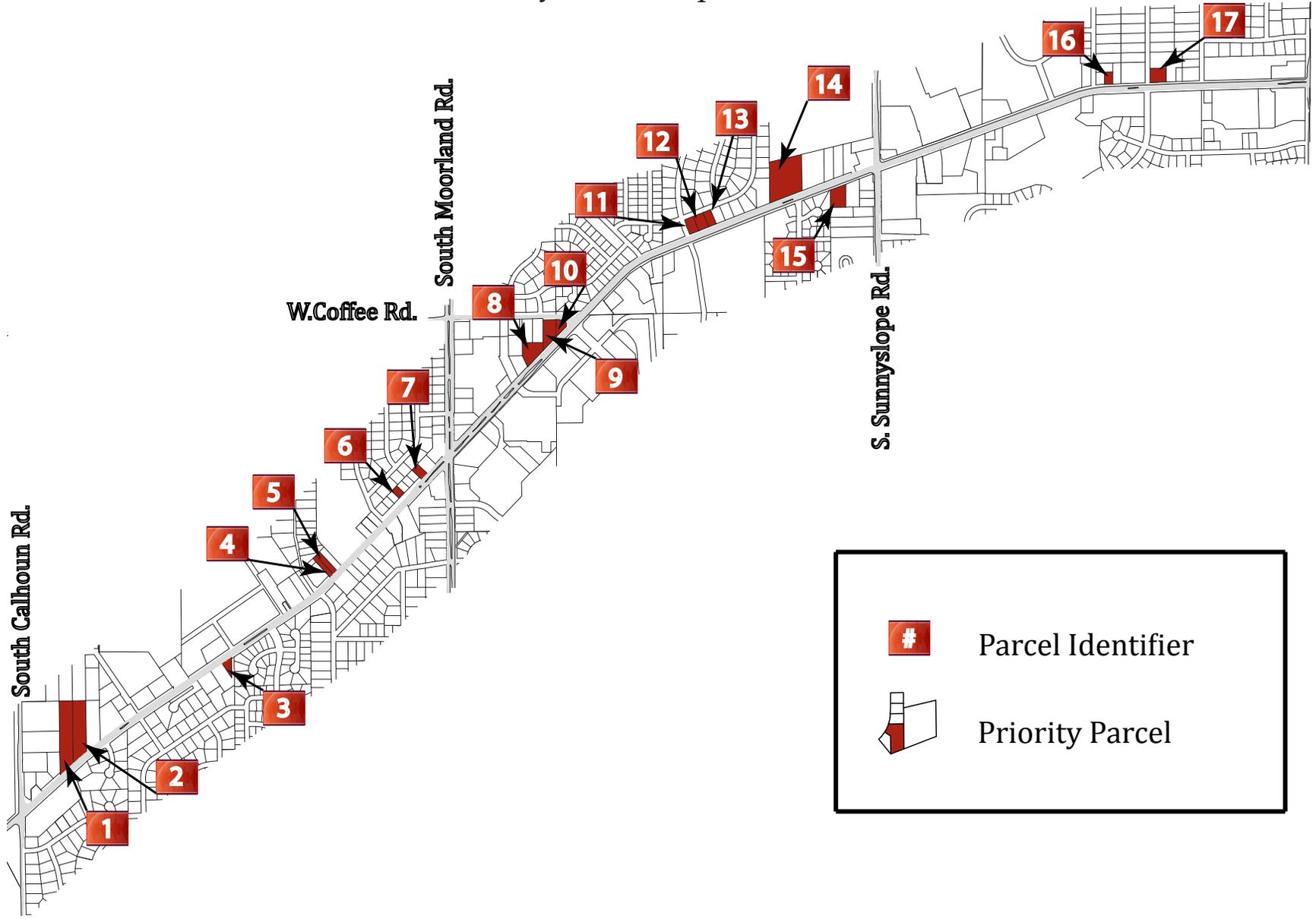
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A.1 Priority Parcels

Several parcels along the East National Avenue Corridor are identified as priority redevelopment parcels. These sites are of particular interest to the promotion and development of the corridor. The parcel identifiers match the site descriptions that follow on the next pages. Color coding in the following descriptions correlates to the regulating zone within the corridor.

1. 17020 West National Avenue
2. 16980 West National Avenue
3. 16405 West National Avenue
4. 16060 West National Avenue
5. 16040 West National Avenue
6. 15752; 15748; 15744 West National Avenue
7. 15700 West National Avenue
8. 15300 West National Avenue
9. 15250 West National Avenue
10. 15140 West National Avenue
11. 14700 West National Avenue
12. 14666 West National Avenue
13. 14636 West National Avenue
14. 14300 West National Avenue
15. 14151 West National Avenue
16. 13196 West National Avenue
17. 12940 West National Avenue

Priority Redevelopment Parcels



Site #1 Identification



East National Avenue Corridor	
Business Type/Name:	Meyer Lumber
Address:	17020 W National Avenue
Location Number:	16
Tax Parcel Number:	NBC 1238990
Site Area:	North side of W National Avenue
General Use:	Retail

Site #2 Identification



East National Avenue Corridor	
Business Type/Name:	Nature's Garden Center
Address:	16980 W National Avenue
Location Number:	15
Tax Parcel Number:	NBC 1238991
Site Area:	North side of W National Avenue
General Use:	Retail

Site #3 Identification



East National Avenue Corridor	
Business Type/Name:	National Auto Body Sales & Services
Address:	16405 W National Avenue
Location Number:	11
Tax Parcel Number:	NBC 1238966 & NBC 1238967
Site Area:	South side of W National Avenue
General Use:	Retail

Site #4 Identification



East National Avenue Corridor	
Business Type/Name:	Guthrie Incorporated
Address:	16060 W National Avenue
Location Number:	14
Tax Parcel Number:	NBC 1237988
Site Area:	North side of W National Avenue
General Use:	Retail

Site #5 Identification



East National Avenue Corridor	
Business Type/Name:	New Berlin Pool & Spa
Address:	16040 W National Avenue
Location Number:	12
Tax Parcel Number:	NBC 1237989
Site Area:	North side of W National Avenue
General Use:	Retail/Residential Mixed Use

Site #6 Identification



East National Avenue Corridor	
Business Type/Name:	Viking Catering, Doctors T.V.; Charles Strehlow, D.D.S.
Address:	15752; 15748; 15744 W National Avenue
Location Number:	7
Tax Parcel Number:	NBC 1212068
Site Area:	North side of W National Avenue, frontage road
General Use:	Retail and Office

Site #7 Identification



East National Avenue Corridor	
Business Type/Name:	New Berlin Barber
Address:	15700 W National Avenue
Location Number:	10
Tax Parcel Number:	NBC 1212995
Site Area:	North side of W National Avenue
General Use:	Retail—Service

Site #8 Identification



East National Avenue Corridor	
Business Type/Name:	True Value
Address:	15300 W National Avenue
Location Number:	8
Tax Parcel Number:	NBC 1207997001
Site Area:	North side of W National Avenue
General Use:	Retail

Site #9 Identification



East National Avenue Corridor	
Business Type/Name:	New Berlin Funeral Home
Address:	15250 W National Avenue
Location Number:	17
Tax Parcel Number:	NBC 127997
Site Area:	North side of W National Avenue
General Use:	Institutional

Site #10 Identification



East National Avenue Corridor	
Business Type/Name:	Mrs. Sippy's & Classic Cuts
Address:	15140 W National Avenue
Location Number:	7
Tax Parcel Number:	NBC 1207998
Site Area:	NWC of W National Avenue & Coffee Road
General Use:	Retail—Service

Site #11 Identification



East National Avenue Corridor	
Business Type/Name:	Casey Chiropractic Clinic & State Farm Insurance
Address:	14700 W National Avenue
Location Number:	4
Tax Parcel Number:	NBC 1205024
Site Area:	NEC of W National Avenue & S 147th Street
General Use:	Office

Site #12 Identification



East National Avenue Corridor	
Business Type/Name:	Ride Sports
Address:	14666 W National Avenue
Location Number:	3
Tax Parcel Number:	NBC 1205025
Site Area:	North side of W National Avenue
General Use:	Retail

Site #13 Identification



East National Avenue Corridor	
Business Type/Name:	PFAFF
Address:	14636 W National Avenue
Location Number:	2
Tax Parcel Number:	NBC 1205989
Site Area:	North side of W National Avenue
General Use:	Retail

Site #14 Identification



East National Avenue Corridor	
Business Type/Name:	Vacant Land
Address:	14300 W National Avenue
Location Number:	5
Tax Parcel Number:	NBC 1205998001
Site Area:	Northwest of W National Avenue & Glen Park Road
General Use:	Vacant

Site #15 Identification



East National Avenue Corridor	
Business Type/Name:	Vacant Land
Address:	14151 W National Avenue
Location Number:	6
Tax Parcel Number:	NBC 1205978
Site Area:	South side of W National Avenue
General Use:	Vacant

Site #16 Identification



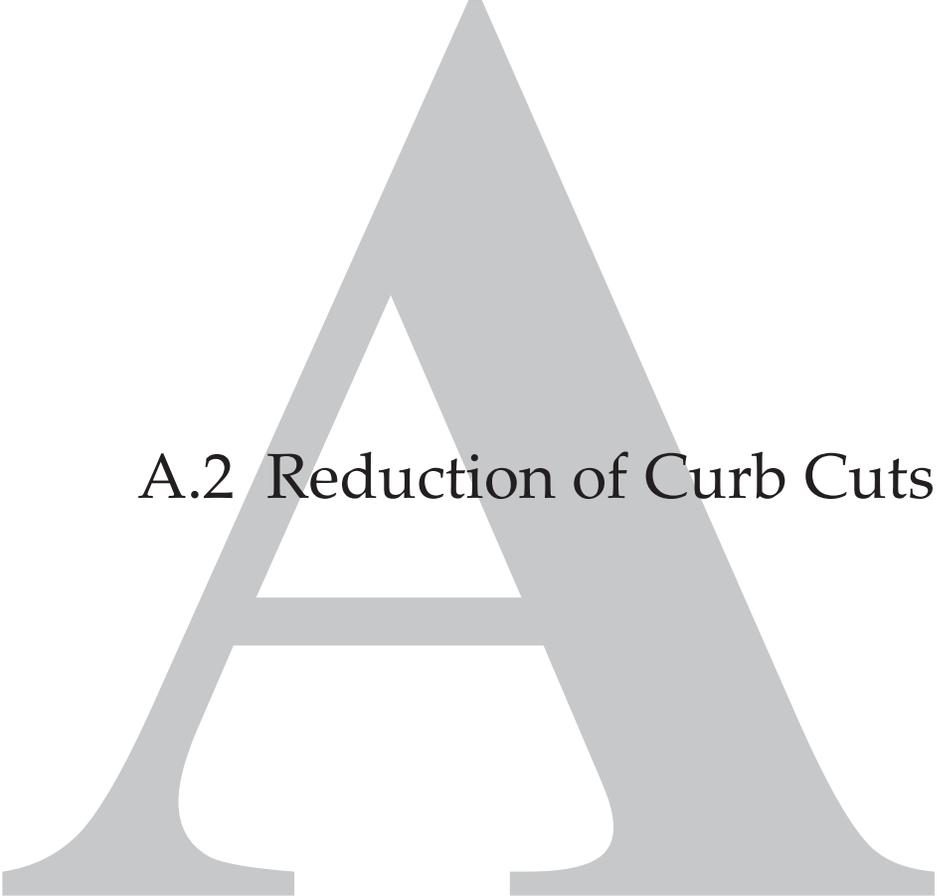
East National Avenue Corridor	
Business Type/Name:	Trident Sales and Services
Address:	13196 W National Avenue
Location Number:	14
Tax Parcel Number:	NBC 1200948
Site Area:	North side of W National Avenue
General Use:	Retail

Site #17 Identification



East National Avenue Corridor	
Business Type/Name:	L & S Liquor
Address:	12940 W National Avenue
Location Number:	1
Tax Parcel Number:	NBC 1200905
Site Area:	NEC of W National Avenue & S 130th Street
General Use:	Retail





A.2 Reduction of Curb Cuts



Curb Cut Reduction

This illustration demonstrates how to implement shared parking lots.

1:

This is an example of several uses sharing one rear access drive. Only one access point is necessary to serve these buildings, while access is also provided to the neighboring parking lot. This layout is a preferred solution since it requires no additional access to the primary street (in the image, Moorland Road).

2:

Here, only one access point interrupts the primary street (in the image, National Avenue) for three separate buildings. In addition, the shared parking lot at the rear of the buildings allows access to a secondary street (in the image, Coffee Road). This system is preferred since it limits curb cuts on National Avenue and promotes the use of secondary roads.

3:

This example shows two existing buildings which each use a separate driveway to reach a main street (National Avenue and Moorland Road). This layout is not desirable, as it doubles the need for curb cuts. The preferred method would share one single access point to either of the main roads, but not both.





Typical Peak Parking Periods For Various Land Uses

Weekday	Evening	Weekend
Banks and public services Offices and other employment centers Park & Ride facilities Schools, daycare centers and colleges Factories and distribution centers Medical clinics Professional services	Auditoriums Bars and dance halls Meeting halls Restaurants Theaters Hotels	Religious institutions Parks Shops and malls

This table indicates peak parking demand for different land use types. Parking can be shared efficiently by land uses with different peaks.

source: *Parking Management - Strategies, Evaluation and Planning* by Todd Litman Victoria Transport Policy Institute. 5 November 2008

A large, light gray, serif capital letter 'A' is centered on the page. The letter is filled with a solid gray color and has a white outline. Inside the white outline of the letter, the text 'A.3 Business Survey' is written in a black, serif font.

A.3 Business Survey



March 12, 2010

Dear National Avenue Business / Property Owner,

The City of New Berlin, in coordination with Urban Planning Graduate Students from UW-Milwaukee, is in the process of creating a regulating plan for National Avenue, from 124th Street to Calhoun Road. Some of you may recall several years ago, the City coordinated with businesses and residents located along the West National Avenue Corridor (the intersection of Moorland Road and National Avenue extending to South Town Road) in the preparation of the West National Avenue Redevelopment Plan, which focused on this smaller area. As a result of the Comprehensive Planning process, additional work was determined to be necessary for the balance of the corridor because many of the issues still remained, such as irregular lot sizes, poor parking & circulation, aging building stock and a lack of cohesive design standards.

Public input is an essential tool to gain insight into the current and future needs of businesses along the National Avenue Corridor. Your response to this survey is important! By completing and returning this survey, you will be providing valuable information which the City will use as it plans for the future.

This survey should take a few minutes. All responses will be kept confidential. If you have any questions regarding the survey, please feel free to contact the Department of Community Development at (262) 797-2445 to speak with a Planner.

Please complete and return this survey by Friday, March 26, in the enclosed postage-paid envelope.

You are also invited to attend an Open House on Tuesday, April 6th from 6:00 to 8:00 PM, located at the New Berlin Library. We will be presenting the findings of this survey and of the recently adopted Comprehensive Plan. Refreshments will be provided!

Thank you for your valuable input!

Sincerely,

Department of Community Development

UW-Milwaukee



National Avenue Business Survey

1. What is your current location in the Corridor?

East (130th St to 147th St)

Central (147th to 159th St)

West (Observatory Rd to Calhoun Rd)

None of the above

2. What type of land use would you consider your property to be?

Residential

Commercial

Institutional

3. Do you currently own or lease the space at your current location?

Own

Lease

4. How many years has your business/property been located along National Avenue?

Less than 1 year

2-5 years

6-10 years

11-15 years

16 or more years

5. Are you planning on any type of improvement for your business at your current location, such as a renovation, parking lot expansion and/or façade improvement?

Yes

No

a) If yes, what type of work are you planning to do and what is the timeline for doing so?

6. If you wish to expand your business, would you consider staying at your present location?

Yes

No

a. If No, what do you believe are your constraints (Please check all that apply)?

Setbacks Requirements

Green Space Requirements

Landscaping / Buffer Requirements

Parking Requirements

Financial Feasibility / Costs

Actual Lot Size (Please see next question)

Other, please explain: _____

7. Does your business have any plans to leave your current National Avenue location?

Yes, but I plan on staying in the Corridor

Yes, I will be leaving the Corridor

No, I don't have any plans to leave

a) If yes, why are you changing locations?

8. Does your business currently have an issue with insufficient parking spaces for customers or employees?

Yes

No

a) If yes, how often?

Every day

1-2 days per week

Just weekends

Seasonally

b) If yes, why do feel you have insufficient parking?

Just not enough spaces

Lot design

Road access

Other: _____

9. Would you be willing to share combined parking facilities with adjoining properties, to reduce maintenance costs but still retain an adequate number of spaces for business use?

Yes

No

If no, please explain why _____

10. A **Business Improvement District** is a special assessment district that can be set up under Wis. Stats. 66.1109 to allow the city and the businesses to share the responsibilities and benefits of improving a well-defined business area. The idea in creating a BID is to fund improvements that attract more customers, enable businesses to gain higher profits and enable the City to eventually gain more tax revenue from higher real estate assessments. BID monies have been used successfully for cross walks, landscaping/streetscape treatments, parking and façade improvements in other communities.

Are you interested in the creation of a Business Improvement District for the corridor?

Yes

No

Maybe

Need more info

11. If financial assistance were available, would you be interested in any of the following economic development programs? (Check any that you would be interested in.)

1. Business Improvement, Small Business Planning (Business seminars/fairs, business skills and management training for businesses and Preparing Small Business Plans)
2. Business Improvement Districts (Defined in question #10)
3. Façade improvement Grants
4. Marketing

12. Overall, what is your image or perception of the National Avenue corridor?

13. In what ways do you think the City could improve upon the corridor?

Create an attractive gateway to the City (124th and National Avenue) to include coordinated landscaping and lighting treatments

Create attractive entryways at specific commercial areas/nodes

Develop solutions to traffic and parking challenges

Create additional bicycle and pedestrian connections between residential and commercial areas along the corridor

Ensure infrastructure (i.e., streets, sewer and water) is upgraded and functioning properly

Encourage developers to assemble parcels for a coordinated development and/or expansions.

14. Would you be interested in following economic development activities in the City through social media websites such as Facebook, LinkedIn, and/or Twitter?

Yes

No

a) If yes, what types of information would you like to see made available through those sites? _____

Thank you for your time in completing this survey!
Your participation will help plan for the future of National Avenue.

**Please place completed survey in the enclosed, postage-paid envelope
and return by Friday, March 26, 2010.**

All survey recipients are invited to attend an Open House on Tuesday, April 6th from 6:00 to 8:00 PM, located at the New Berlin Library. We will be presenting the findings of this survey and of the recently adopted Comprehensive Plan.

Business Survey Results

What is your current location in the Corridor?

- 25.3% East (130th St to 147th St)
- 6.3% Central (147th to 159th St)
- 50.6% West (Observatory Rd to Calhoun Rd)
- 17.7% None of the above

What type of land use would you consider your property to be?

- 0% Residential
- 1.3% Institutional
- 98.7% Commercial

Do you currently own or lease the space at your current location?

- 49.4% Own
- 50.6% Lease

How many years has your business/property been located along National Avenue?

- 6.3% Less than 1 year
- 13.9% 2-5 years
- 13.9% 6-10 years
- 12.7% 11-15 years
- 53.2% 16 or more years

Are you planning on any type of improvement for your business at your current location, such as a renovation, parking lot expansion and/or façade improvement?

- 10.4% Yes
- 89.6% No

If you wish to expand your business, would you consider staying at your present location?

71.4% Yes
28.6% No

Constraints Contributing to Leaving*

	Would contribute	Would not contribute
Setbacks Requirements	10%	90%
Green Space Requirements	20%	80%
Landscaping / Buffer Requirements	15%	85%
Parking Requirements	25%	75%
Financial Feasibility / Costs	35%	65%
Actual Lot Size	15%	85%
Other	35 %	65%

Percentage represents only 28.6% who said they would not stay.

Does your business have any plans to leave your current National Avenue location?

- 5.3% Yes, but I plan on staying in the Corridor
- 6.6% Yes, I will be leaving the Corridor
- 88.2% No, I don't have any plans to leave

Does your business currently have an issue with insufficient parking spaces for customers or employees?

- 73.1% No
- 26.9% Yes

How Often*

Time Frame	Percent
Every day	60%
1-2 days per week	15%
Just Weekends	15%
Seasonally	3%

Causes of the Insufficient Space*

Cause	Percent
Not enough spaces	60%
Lot design	33%
Road access	0%
Other	7%

* Percentages represent only the 26.9% that reported insufficient space



Would you be willing to share combined parking facilities with adjoining properties, to reduce maintenance costs but still retain an adequate number of spaces for business use?

50.7% Yes

49.3% No

A **Business Improvement District** is a special assessment district that can be set up under Wis. Stats. 66.1109 to allow the city and the businesses to share the responsibilities and benefits of improving a well-defined business area. The idea in creating a BID is to fund improvements that attract more customers, enable businesses to gain higher profits and enable the City to eventually gain more tax revenue from higher real estate assessments. BID monies have been used successfully for cross walks, landscaping/streetscape treatments, parking and façade improvements in other communities.

Are you interested in the creation of a Business Improvement District for the corridor?

14.5% Yes

32.9% No

11.8% Maybe

40.8% Need more info

If financial assistance were available, would you be interested in any of the following economic development programs?

Program	Interested	Not Interested
Business Improvement, Small Business Planning	25%	75%
Business Improvement Districts	17%	83%
Façade improvement Grants	26%	71%
Marketing	30%	70%

In what ways do you think the City could improve upon the corridor?

Method of Improvement	Could Improve	Could Not Improve
Create an attractive gateway to the City (124 th and National Avenue) to include coordinated landscaping and lighting treatments	22%	78%
Create attractive entryways at specific commercial areas/nodes	22%	78%
Develop solutions to traffic and parking challenges	35%	65%
Create additional bicycle and pedestrian connections between residential and commercial areas along the corridor	14%	86%



Ensure infrastructure (i.e., streets, sewer and water) is upgraded and functioning properly	34%	76%
Encourage developers to assemble parcels for a coordinated development and/or expansions	33%	77%

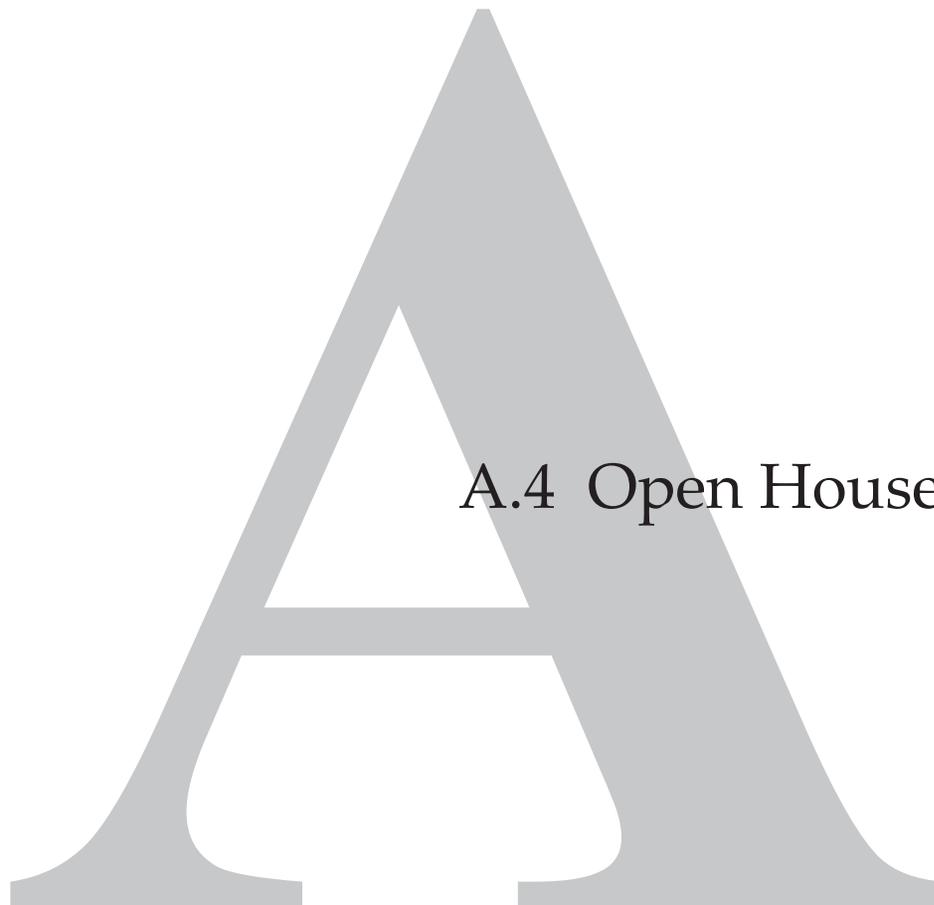
Would you be interested in following economic development activities in the City through social media websites such as Facebook, LinkedIn, and/or Twitter?

23% Yes

77% No

Selected Responses from Business Survey Views on National Avenue Corridor Character

1. "Nothing Uniform."
2. "Not all up to date."
3. "There is nothing to distinguish it and proclaim it to be 'the downtown area.'"
4. "Typical business area. Nothing unique or charming."
5. "It needs a lot of help! It needs a more elegant look."
6. "Disorganized."
7. "Lacks personality."
8. "Poor traffic patterns and lot situations."
9. "Its fine."
10. "Excellent location."
11. "Very positive."
12. "Corridor is currently acceptable."
13. "Getting better"
14. "Old meets new."
15. "Very well kept."



A.4 Open House

Two node designs were presented at the public Open House on April 6, 2010. The following pages are samples of what was shown to the public for comment. The comments were reviewed after the meeting and the designs revised, which are reflected in Chapter 5.4.

COFFEE ROAD - OPTION 1

This option focuses future development along Coffee Road at National Avenue. The buildings line the street and are situated perpendicular to the street edge. A shared parking lot is at the rear of the buildings, with access from both streets. Due to the large area of parking, surface runoff is filtered at the adjacent stormwater detention pond. Street trees and furniture complement the pedestrian realm. Buildings are scaled to complement the City Center development on National Avenue, and also not to overpower the residential area north of Coffee Road. This option offers 68,000 of retail space.



BIRD'S EYE VIEW OF FUTURE DEVELOPMENT. View towards west at Coffee Road and National Avenue intersection.

April 6, 2010



View of National Avenue access to parking in rear of building.



View of new building with corner plaza at Coffee Road and National Avenue.



View east down National Avenue, with City Center across the street.

CHARACTER IMAGES



COFFEE ROAD - OPTION 2

This future development option offers a concentration of new buildings at the Coffee Road and National Avenue intersection. The new buildings line the street to provide a continuous street edge. Street trees are also planted continuously, which will shade pedestrians and the buildings in the summer. Parking is located to the rear of each building, with access points from both Coffee Road and National Avenue. Stormwater will drain to a large bioretention area adjacent to the parking lot, where it will be filtered with native vegetation and not add capacity to the municipal system. This option offers approximately 50,000 square feet of retail or office space.



View of new building with corner plaza at Coffee Road and National Avenue.



BIRDS EYE VIEW OF FUTURE DEVELOPMENT OPTION. New building hugs the corner at Coffee Road and National Avenue; building scale and materials will mimic that of City Center across the street.

April 6, 2010



CHARACTER IMAGES



Example of porous pavement which allows water to filter back into the soil and recharge the storm sewer system.



EXAMPLE BIORETENTION AREA. Parking lot sheet drains across pavement toward retention area collecting pollutants along way. Plants such as native grasses planted in the bioswale filter the stormwater before returning it to recharge the groundwater supply.



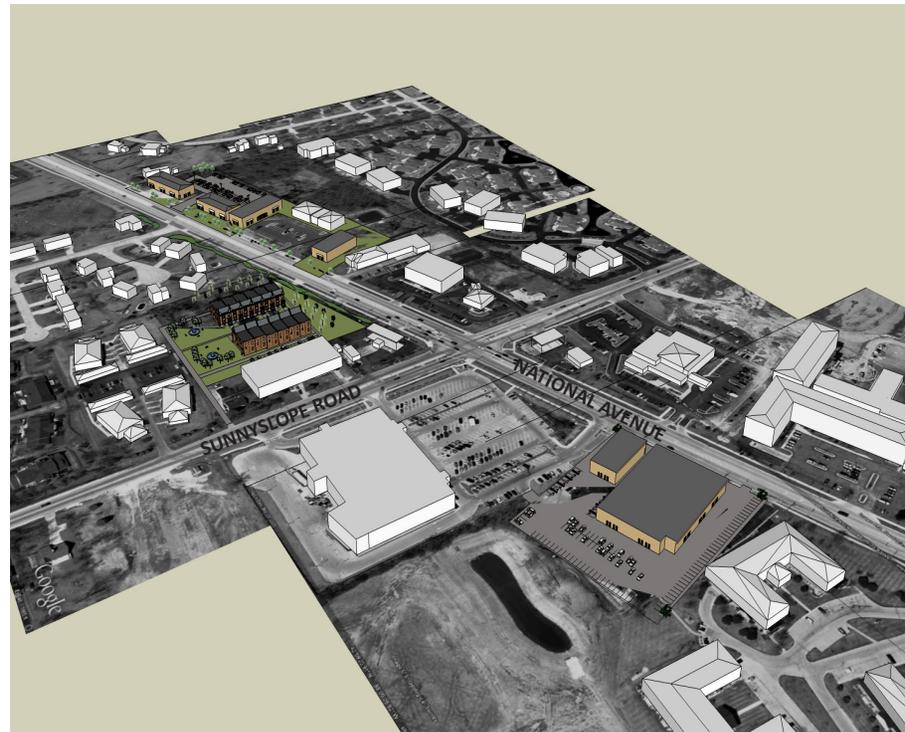
SUNNYSLOPE ROAD - OPTION 1

The Sunnyslope Road Node will encompass additional complementary retail along National Avenue both east and west of Sunnyslope Road. Retail buildings will address the street edge of National Avenue with parking along the side and rear of the new buildings. The two retail buildings located on the eastern portion of the node will share access with the adjacent Pick 'N Save grocery store.

Option 1 will include two multi-family residential buildings near the southwestern corner of Sunnyslope Road and National Avenue. The buildings will be positioned perpendicular to National Avenue. The entrance to the residential units will be located along Sunnyslope Road to help minimize negative traffic impacts. A buffer will be placed between National Avenue and the residential units to transition from the public right of way. Parking for the residential units will be located along the back side of each building.



View of new commercial buildings west of Sunnyslope Road.



Birds eye view looking northwest at the intersection of Sunnyslope Road and National Avenue.

April 6, 2010



View of multi-family buildings at Sunnyslope Road and National Avenue.



Buildings on National Avenue. The buildings existing in this area could remain, yet receive facade improvements.

SUNNYSLOPE ROAD - OPTION 2

The Sunnyslope Road Node will encompass additional complementary retail along National Avenue both east and west of Sunnyslope Road. Retail buildings will address the street edge of National Avenue with parking along the side and rear of the new buildings. The two retail buildings located on the eastern portion of the node will share access with the adjacent Pick 'N Save grocery store.

Option 2 will include a new commercial building located at the southwest corner of Sunnyslope Road and National Avenue. The front of the building will address National Avenue. The site will utilize two entry points, one along National Avenue and the other along Sunnyslope Road.



View of new commercial buildings west of Sunnyslope Road.



View of new commercial buildings east of Sunnyslope Road.



Birds eye view looking northwest at the intersection of Sunnyslope Road and National Avenue.

April 6, 2010



View of new commercial building west of Sunnyslope Road. This building is parallel to the road edge and features parking in the rear.

CHARACTER IMAGES



WESTERN GATEWAY - OPTION 1

The Western Gateway Option 1 maintains residential land uses rather than retail or commercial development. Senior living apartments scatter the outer edges of the node, while single family town homes are found inside the development. Small retail stores or commercial businesses line National Avenue. Parking for the senior living apartments and businesses is located inside the development, away from the street. This allows for better traffic flow and less congestion on National Avenue.



View facing southeast, near the corner of Observatory Road and Calhoun Road. National Avenue borders the top of the photo while Calhoun Road is located directly in the foreground.

April 6, 2010



CHARACTER IMAGES



Example of possible town homes.



Example of possible senior community.



The Western Gateway Node acts as a gateway into the city's commercial district. Gateways can help to define important spaces or mark an area's beginning or end. There are various ways to create a gateway. Decorative walls, marker signs, flower planters, fountains, or sculptures could all be used in order to define a gateway. The corner of National Avenue and Calhoun Road is a gateway that marks where the more urban, east half of New Berlin meets the more rural, western half of the city.



Example of possible commercial.

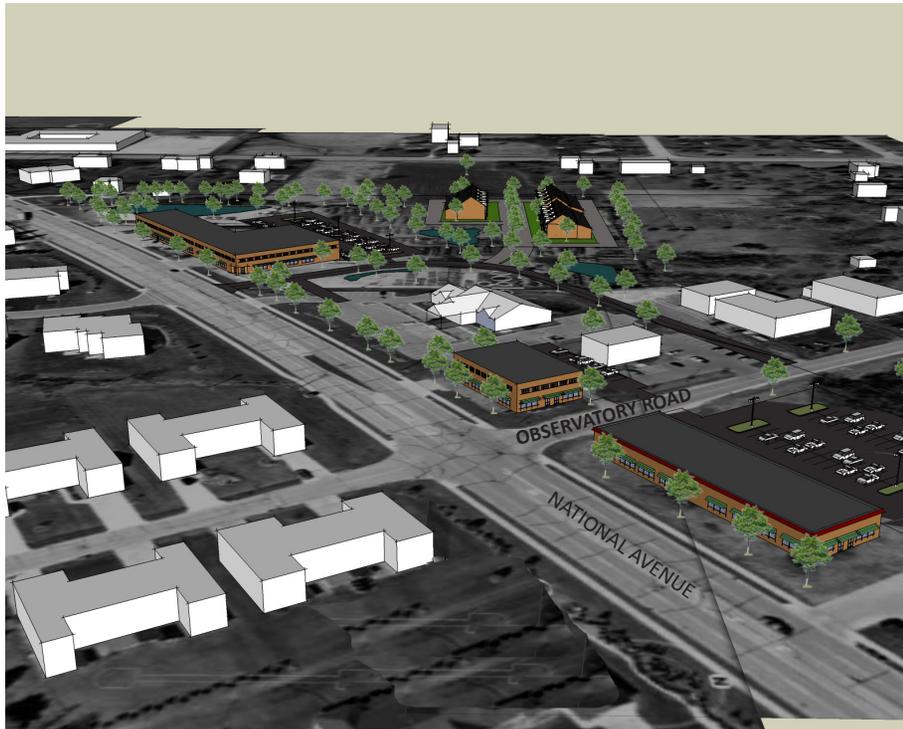


Example of possible public square/plaza.



WESTERN GATEWAY - OPTION 2

The Western Gateway Option 2 maintains existing single family housing along Calhoun Road. The remaining land is a mixture of retail, office and commercial development. A possible condominium or co-housing development is placed to the rear of National Avenue. Retail and office space lines National Avenue with a pleasant tree line and uniform setback. Parking for any retail and office uses is placed behind the buildings. Parking lot access is shared with adjacent business when possible. The number of parking lot driveways along National Avenue is reduced to allow for better traffic flow and ease congestion.



Bird's eye view looking west along National Avenue at Observatory Road.

April 6, 2010



Retail space with parking along the side of the building and parking lands with green space.



Retail along National Avenue with trees along parkway.



View of new multifamily housing, with pond providing water retention and landscaping feature.



New building with first floor retail space and second floor office space on National Avenue.

CHARACTER IMAGES



Residential street with bio-swale along curb with natural plantings to filter stormwater.



Example of a co-housing development, which could possibly be an active senior community.

